

Masterplan Design and Access Statement B&Q Cricklewood, Cricklewood Lane

Montreaux Cricklewood Developments Ltd
July 2020



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MEP & Utilities	Meinhardt Group
Energy & Sustainability	Meinhardt Group
Structural	Meinhardt Group
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Waste	AECOM
Ecology	AECOM
Civils	AECOM
Water Resources, Flood Risk & Drainage	AECOM
Archaeology	AECOM
Socio Economics	AECOM
Air Quality	AECOM
Climate Change	AECOM
Ground Conditions & Contamination	AECOM
Noise and Vibration	AECOM
Rapid Health	AECOM
Arboriculture	AECOM
Visuals - Verified Views	Cityscape
Cost Consultancy	WWA
Construction	Stace

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06	Design team and legal review	2020.07.28	SN	JE
07	Final draft	2020.07.29	SN	JE
08	For Submission	2020.07.31	SN	JE



1 Introduction





1.1 The opportunity

The proposals for B&Q Cricklewood have been carefully designed to complement and enhance the existing community by transforming this under-used parcel of land into a new, thriving community forming an integral part of the town centre.

Potential for significant improvement to public realm, permeability and legibility to the wider area means that the Proposed Development will respond positively to its emerging urban context by providing a number of local benefits including enhanced pedestrian and cycle routes through the Site and new public open spaces.

The Site, situated within the Cricklewood/Brentcross Opportunity Area and Edgware Road Corridor of Change, provides the opportunity to transform this brownfield site and provide housing in light of significant need within the Local Borough of Barnet (LBB). The draft London Plan sets out a minimum ten-year housing target of 31,340 residential dwellings for LBB. Additionally, development of the Site will help to alleviate pressure for LBB to find additional land for development within the Metropolitan Green Belt.

The proximity of the Site to the existing Cricklewood train station and town centre, when considered in combination with local and national planning policies and existing local conditions, makes the Site an ideal sustainable location for a new higher density residential-led development with direct connections into Central London.

High PTAL rating across the Site – PTAL 4 and PTAL 5 - signify it is a highly sustainable location for residential-led intensification (as specifically advocated within the adopted and draft London Plan).

The site is within easy, quick and direct walking distance to Cricklewood town centre – providing a pivotal opportunity for increasing pedestrian connectivity and legibility to and from the Site and the surrounding area (which is currently lacking).

1.2 Purpose of this document

This Masterplan Design and Access Statement has been prepared by EPR Architects on behalf of Montreaux Cricklewood Developments Ltd in support of an outline planning application for the B&Q Cricklewood site.

The purpose of this Masterplan Design and Access Statement is to set out the evolution of the Illustrative Masterplan and general design principles. Fundamentally, this document shows that the Masterplan has been consulted and developed with LBB in a progressive and collaborative manner, and is appropriate for the quality and amount of development being applied for.

The Masterplan Design and Access Statement should be read in conjunction with:

- Parameter Plans; and
- Design Guidelines

Future Reserved Matters applications should be brought forwards in line with the standards set out in the Design Guidelines.



Adjacent Sketch aerial view of the Illustrative Masterplan

1.3 Planning submission structure

This outline application for the B&Q Cricklewood site is comprised of the following documents submitted for approval:

- Masterplan Design and Access Statement;
- Design Guidelines; and
- Parameter Plans

1.4 Scope of the planning application

Outline planning application (including means of access with all other matters reserved) for the demolition of existing buildings and comprehensive redevelopment of the Site for a mix of uses including residential C3 and flexible commercial and community floorspace in uses classes A3/B1/D1 and D2; car and cycle parking; landscaping; and associated works.

It is anticipated that this project will be built out as a phased development. This outline application seeks approval for access.

External appearance, layout, scale and landscaping are reserved for future consideration, as part of a future Reserved Matters Application (RMA) submitted for the Development Parcels (in line with the Parameter Plans).

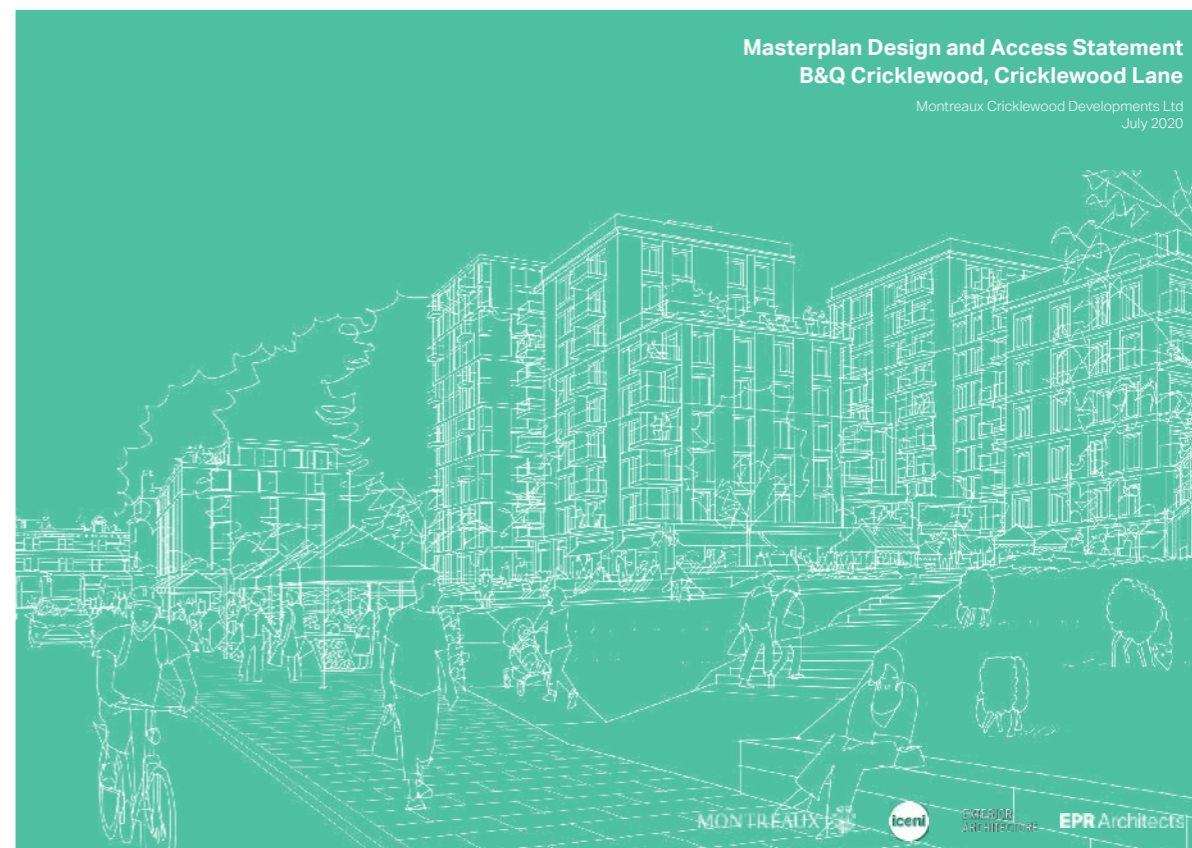
An Illustrative Masterplan is provided as part of this application, demonstrating one way the Proposed Development could be delivered in accordance with the Parameter Plans, Masterplan Design and Access Statement and Design Guidelines.

The Illustrative Masterplan establishes a vision for the Site and demonstrates the potential of the Site delivering the amount and density of development proposed.

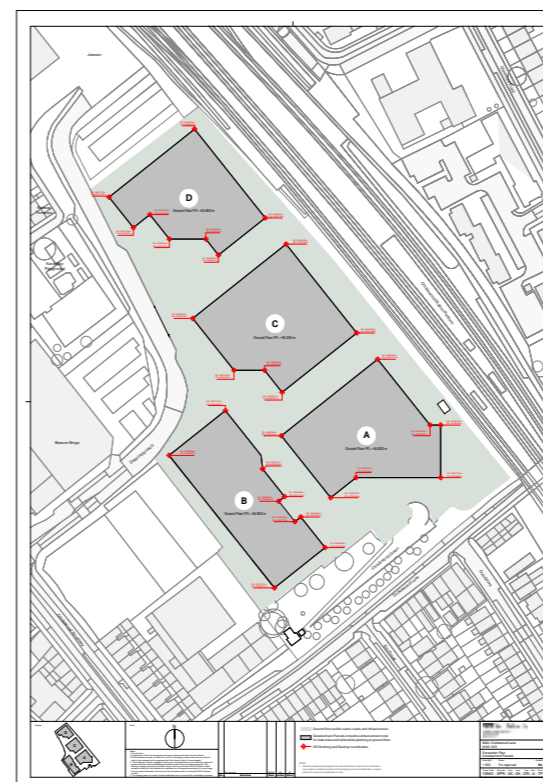
The Illustrative Masterplan has been designed to take account of the following :

- Site context, opportunities and constraints;
- Masterplan design and access principles;
- Planning policy;
- Planning consultation feedback;
- Environmental assessments; and
- Public exhibition and consultation feedback.

Please refer to Table 1.1 in the Planning Statement for all application documents.



Masterplan Design and Access Statement



Parameter Plans



Design Guidelines

1.5 Montreaux Developments Ltd

Montreaux Developments Limited is a mixed use and residential developer that has a proven track record of delivering high quality schemes across London and the south east; creating landmark schemes, delivering new homes and regenerating brownfield sites.

1.6 EPR Architects

EPR Architects is an award-winning architectural studio based in Central London. As one of the largest ranked practices in the AJ100, 2017 and 2018 saw us awarded the AJ100 Clients' Choice Award, recognising our commitment to client satisfaction and design excellence.

With over 70 years of experience in the industry we provide project leadership and expert construction knowledge spanning a range of project typologies from urban regeneration masterplans, global corporate headquarters and progressive industrial parks to prestigious heritage conservations, ultra-luxury residences, boutique hotels and spas, price point residential buildings, serviced apartments and Build to Rent.



Montreaux, Margarine Works, Southall



Montreaux, Indecon Court, Tower Hamlets



Montreaux, Putney Plaza, Wandsworth



EPR Architects, The Garden Quarter, Fulham



EPR Architects, The Ram Quarter, Wandsworth



EPR Architects, Quayside Quarter, Southall





Adjacent Sketch view of Cricklewood Green and the Illustrative Masterplan from Cricklewood Lane



2.1 The Site

This chapter on context analyses key aspects of the Site and its surrounding areas, leading to a comprehensive appreciation of the opportunities for successful development.

The Site is located to the north of Cricklewood Lane and to the west of Cricklewood Railway Station, within an established dense urban area comprising a mixture of uses including residential, retail and commercial. The existing scale and character of buildings varies in the immediate surrounding context resulting in a diverse character in the built environment.

Strategically located between Cricklewood Railway Station and within Cricklewood town centre, the Site benefits from significant passing footfall and good transport connectivity, making it a sustainable location for redevelopment and intensification.

Cricklewood Green, deemed an Asset of Community Value in 2017, lies to the south of the Site and both addresses the change in level beside the Site and Cricklewood Lane and connects Cricklewood Lane to the Site. Retention and enhancement of this locally cherished green space is key to the success of future development.



Pavement in front of Cricklewood Green and B&Q used for market stalls



Existing vehicle entry/exit to B&Q from Cricklewood Lane



Existing pedestrian access to B&Q via Cricklewood Green



Existing vehicle entry/exit to B&Q from Depot Approach

2.2 Planning context

Key policy considerations

The Site itself is not designated for a specific land use in local policy terms. However, it is located at a prominent position along the Edgware Road (A5), “key corridor of change” and within the Brent Cross and Cricklewood Opportunity Area (“OA”) – designated as an area for major housing and employment growth within both LBB’s Local Plan and the London Plan. This objective is further emphasised within the Draft London Plan (Intend to Publish 2019) – signifying the importance of growth within this area, to London as a whole.

The emerging Local Plan for LBB also recognises the importance of redeveloping and intensifying the use of underutilised land between the Broadway and Cricklewood Station, which specifically includes the Site.

Taking account of the adopted and emerging policy position, the Site represents a pivotal brownfield opportunity located directly adjacent to Cricklewood Train Station and District Town Centre with a PTAL of 4-6. In such locations, housing as part of mixed-use proposals should be optimised – recognising the scarcity of brownfield land in London. Indeed, this is specifically recognised within the DLP and evidenced in LBB’s requirement to find additional land for housing by way of the emerging Local Plan. The proposals will help to alleviate pressures on LBB, through enhancing a heavily underutilised site, whilst significantly improving the Site’s outlook, permeability and public realm, to the benefit of the local area. The scheme will provide a mix of housing (including affordable and family units) and supporting commercial space in ensuring the continued vitality of Cricklewood, in tandem with creating an important gateway and sense of arrival into this important part of the Borough. This is in line with national, regional and local policy objectives which should be considered favourably in planning terms.



Aerial view of the B&Q site within wider context

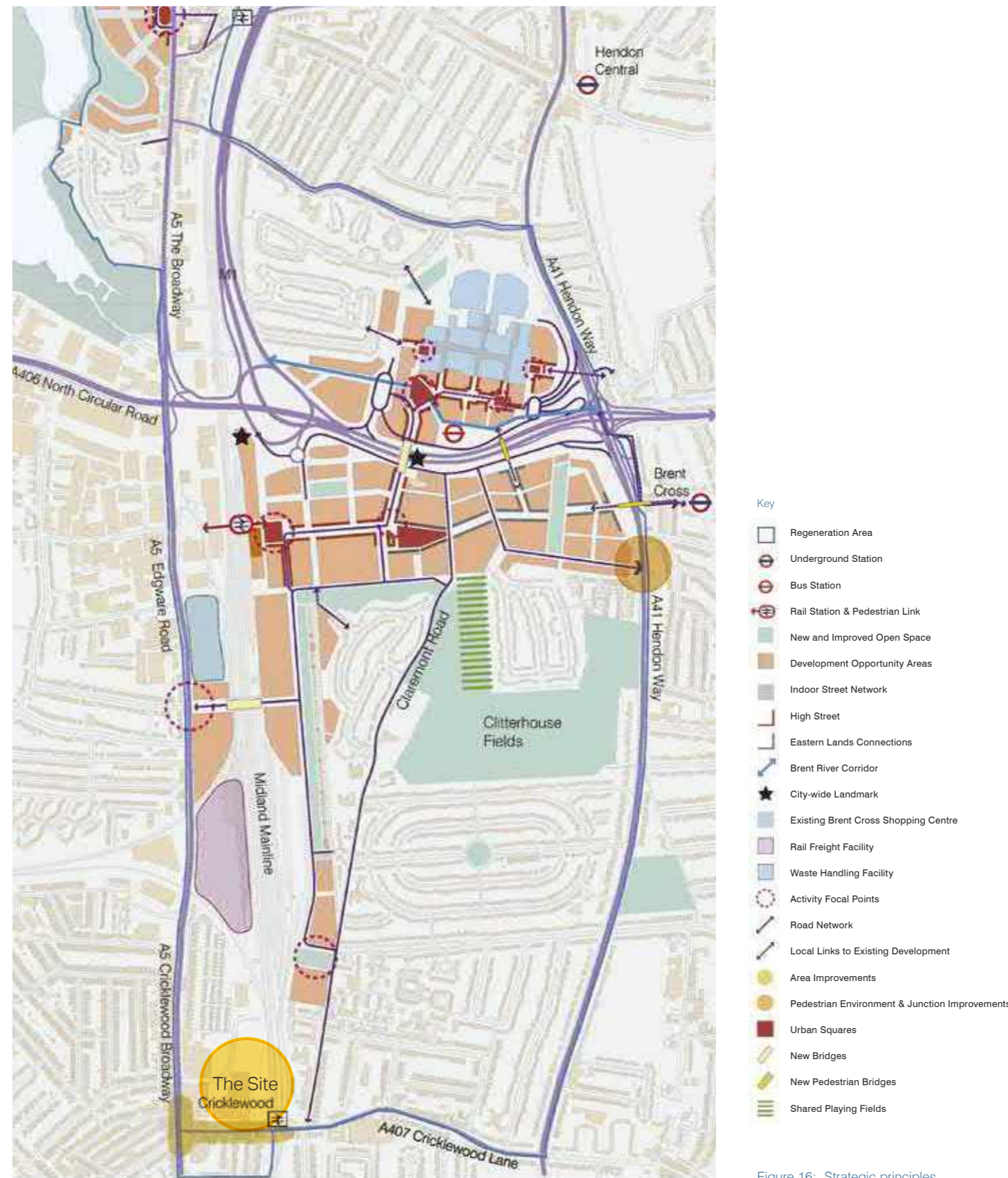
Planning history and emerging site context

The immediate Site area has been subject to significant change in recent years – including within the wider OA area and at neighbouring sites. This includes 1-13 and 194-196 Cricklewood Lane whereby, higher-density, residential-led proposals have recently been considered favourably by LBB. The Masterplan proposals have carefully considered this emerging context, in seeking to complement the changing character and regeneration of the area as whole. This is illustrated within the remaining sections of the Masterplan Design and Access Statement, with further planning justification provided within the Planning Statement (as prepared by Icen Projects).

Planning policy

With regards to the adopted policy position - the Site is designated within the Cricklewood Regeneration and Opportunity Area (as per the London Plan) – designated to deliver significant housing growth of mixed type and tenure, alongside leisure space, retail and community facilities. The wider opportunity area is currently subject to significant growth, for higher-density, residential-led proposals. The Site also lies adjacent to the Cricklewood Town Centre Boundary and train station (PTAL 4-6).

Accordingly, the Site represents a pivotal opportunity for comprehensive redevelopment, of a highly sustainable brownfield site whereby such development is actively supported and encouraged in such locations by way of both the adopted and emerging London Plan. Redevelopment of the Site will also help to significantly boost the supply of new homes within the Borough and act as a catalyst for the wider regeneration of Cricklewood as a whole - which should be wholly supported in planning terms.



Brent Cross Cricklewood Development Framework - Strategic Principles

Figure 16: Strategic principles

Planning history

On review of LBB's online planning register, there is no evidence of applications submitted or approved for the comprehensive redevelopment of the Site. Notwithstanding, the Site lies directly adjacent to a large part of the Opportunity Area, subject to Outline Planning approval in October 2010 for comprehensive mixed-use redevelopment. A number of detailed submissions have since been made in respect of the phase 1 development.

There is also evidence of applications submitted and/ or approved for the residential-led development within the immediate proximity of the Site and more recently at the existing Co-op site, 1-13 Cricklewood Lane – whereby a planning application was submitted on the 23rd October 2018, for erection of 3 residential buildings comprising 187 residential units and 2,115 sq.m of flexible commercial space (Use Classes A1-A4 and D1) at ground floor level fronting Cricklewood Lane. Notably, planning application 18/6353/FUL proposes a 15-storey building in the north east corner of the Site, stepping down to six storeys onto Cricklewood Lane. The proposals also include a mix of studios, one, two-and three-bedroom flats; in addition to 80 car parking spaces at ground and basement level, equating to a ratio of 0.41 spaces per unit, this site has a PTAL rating of 6a and 5 (northern section).

The above signifies the degree of change currently being undertaken within the immediate Site Area, exemplifying the opportunity that the Site provides in terms of delivering complementary development which will go towards ensuring the continued vitality of this area of Cricklewood into the future – which we are aware is key aspiration for LBB; particularly in creating a 'gateway' into this area, adjacent to the train station.



Aerial view of B&Q site with emerging context demarcated.



1-13 Cricklewood Lane (Planning Reference 17/0233/FUL)

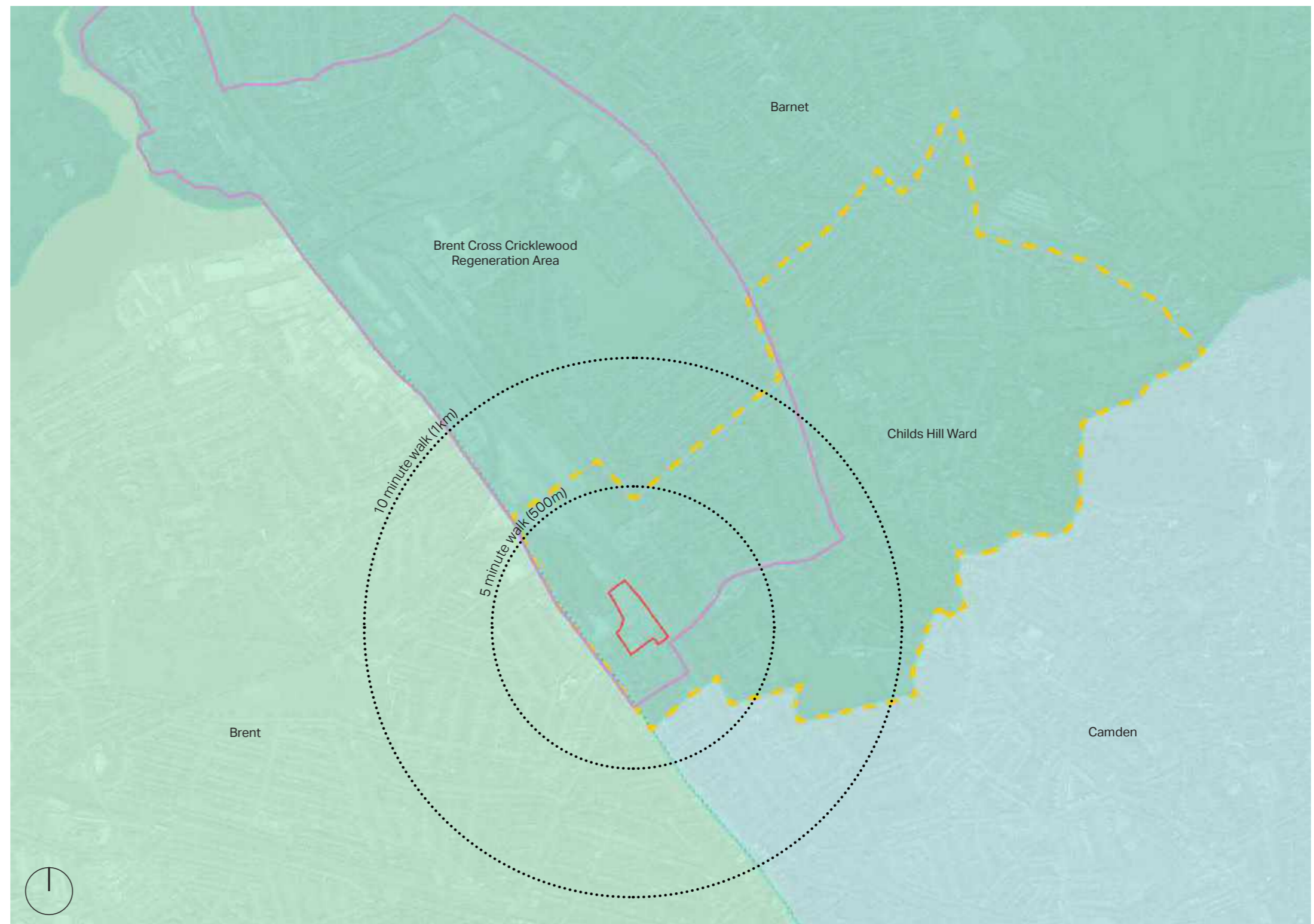


194-196 Cricklewood Broadway (Planning Reference: 18/6353/FUL)

2.5 Site within Barnet

The Site is located within Childs Hill Ward in the London Borough of Barnet (LBB) and lies between Willesden Green and Dollis Hill to the south west, West Hampstead and Childs Hill to the east and Brent Cross to the north.

The town centre of Cricklewood currently does not benefit from its own designated public space, town hall nor library. The public realm, green space and possible community facilities associated with the Proposed Development would benefit the local communities in not just one borough but three.



Map illustrating Ward and Borough boundaries

2.6 Transport connectivity

Rail and Tube

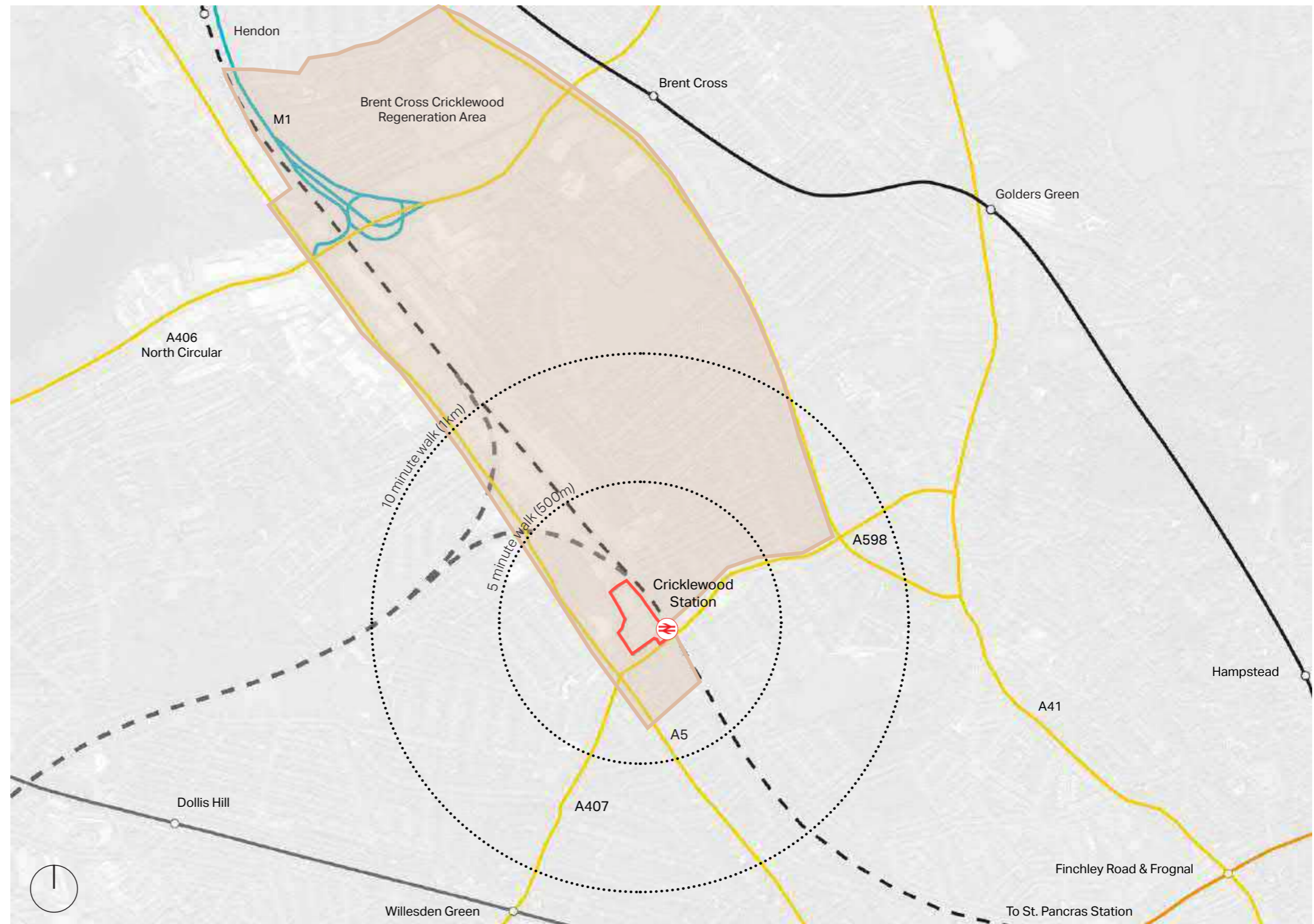
Cricklewood Station (in Zone 3), immediately adjacent to the Site, is the nearest main-line station with Thameslink services to King's Cross St Pancras in 10 minutes, Farringdon station in 16 minutes and Luton Airport in 35 minutes.

Willesden Green and Kilburn stations, which are on the Jubilee line in Zone 2, lie within 15 minutes walk from Cricklewood Broadway. Journeys to Baker Street take 11 minutes and Westminster, 17 minutes.

Road

Cricklewood Broadway (A5) is the main north-south road through the area. It forms part of the Edgware Road leading directly to Marble Arch, between Oxford Street and Hyde Park.

Cricklewood is also well served by buses. It has a bus garage which was completely rebuilt in 2010 and there are frequent bus services to Victoria, Hammersmith, Oxford Street, Brent Cross and Golders Green.



Map illustrating surrounding existing transport networks

- Site
- Motorway
- A-Road
- - - Railway
- Northern Line
- Jubilee Line
- London Overground

2.7 Green infrastructure

With 28% of its area designated as Green Belt and 8% as Metropolitan Open Land, Barnet is one of the greenest boroughs in London.

Barnet has over 200 parks and open spaces, 67 sites of nature conservation importance, and a site of special scientific interest at Welsh Harp Reservoir. There are 1192 hectares of public open space including those parts of Green Belt and MOL, all of which are accessible to the public.

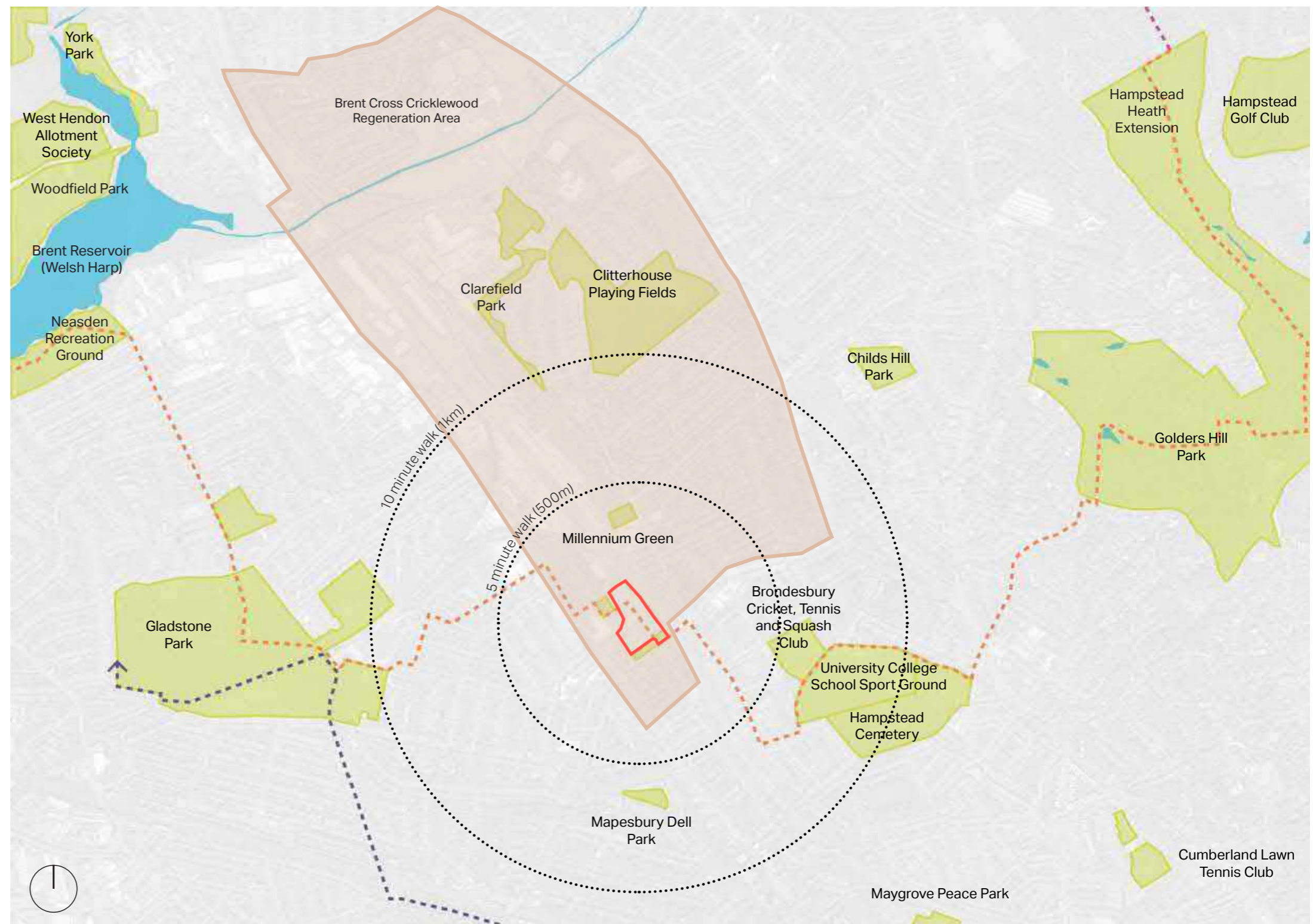
The Site however, does not significantly benefit from these existing open green spaces as most are more than 1km from the Site leaving Cricklewood town centre without meaningful open green space within walking distance apart from Cricklewood Green to the south of the Site.

Dollis Valley Greenwalk

The Dollis Valley Green Walk is a 17 km long strategic walking route connecting Brent Park, Hendon, Windsor Open Space Finchley, Riverside Walk / Gardens, North Finchley, and Brook Farm / Wyatts Farm, Whetstone, providing an important green corridor for wildlife and recreational use through the heart of Barnet and improving the quality and safety of London's parks.

A future extension of the Dollis Valley Greenwalk which connects Cricklewood Town Centre, Gladstone Park, and Neasden Recreation Ground to Section 10 of the Capital Ring at Welsh Harp Open Space and Brent Reservoir is possible through the Site.

- Site
- Green space
- Cycle route Quietway 3 - Regent's Park to Gladstone Park
- Dollis Valley Greenwalk
- - - Potential Extension to Dollis Valley Greenwalk



Map illustrating existing green infrastructure within the wider site context

2.8 Tall buildings

It is recognised in the LBB's Core Strategy (September 2012) that the majority of the approved schemes within this area will involve tall buildings.

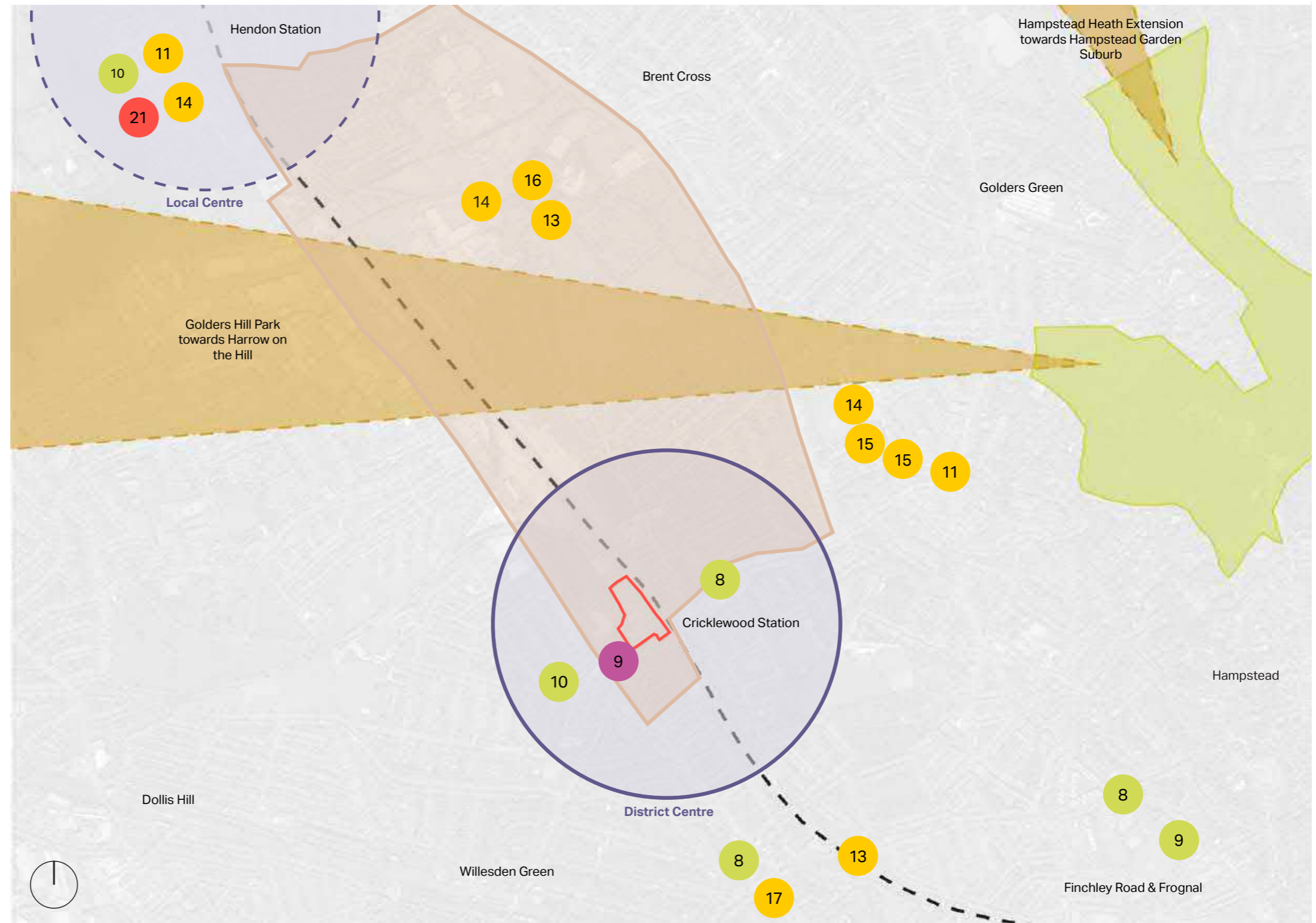
As the Site is within the Brent Cross Cricklewood Regeneration Area, proposed tall buildings on the Site should comply with the urban design principles established in the Brent Cross Cricklewood Development Framework; including:

- *Taller residential buildings will be sensitively located;*
- *Taller buildings within the higher density residential areas will not form the dominant residential typology; and*
- *Residential buildings above 15 storeys should be located along the line of the mainline railway.*

The Brent Cross Cricklewood masterplan maximises the use of the Site in accordance with the London Plan and will result in a change in the urban form that is appropriate in a residential-led mixed use regeneration scheme.

Existing tall buildings are strategically located around public transport nodes and key employment and retail destinations.

The strategy for the location of height on the Site has been informed by the policy position, site characteristics as well as the Site's designation as a District Centre, all supporting the placement of the tallest elements of the Proposed Development closest to the railway and taking into account local viewing corridors.



Map illustrating existing and proposed tall buildings within the wider site context

- Site
- Locally Important Views
- ⊗ Emerging Context Storey Height
- ⊗ Existing Context Storey height
- Railway

2.9 Cricklewood town centre

Cricklewood town centre is defined by the linear retail frontages along Cricklewood Broadway and its branch along Cricklewood Lane towards Cricklewood Station.

Cricklewood is a busy urban neighbourhood with both inner and outer London characteristics.

The period buildings in the town centre are characterised by ornamented brick buildings built in the early 20th century. In contrast, the newer developments are less ornate in appearance and architectural fenestration.

The street pattern of the residential neighbourhood is coherent and regular in the town centre area. However, the railway establishes a strong east-west divide and Cricklewood Lane is one of the key pedestrian links across the railway.



- Cricklewood Station building
- Cricklewood town centre

Map illustrating the extents of Cricklewood town centre and high street in relation to the Site



Existing architecture along the high street, Cricklewood Broadway, characterised by Victorian, Edwardian and modern developments (which range in height from three to five storeys), and ground floor retail units

2.10 Movement and access

Pedestrian and cycle access

There are existing uninterrupted walking and cycling routes stretching from the top of Hyde Park to the Site via Cricklewood Broadway and beyond. However, there are currently no provisions for segregated cycle routes in the surrounding streets.

The proposed masterplan therefore has the potential to provide a safer and more enjoyable pedestrian/cycling experience through the Site, in accordance with the GLA's Healthy Streets principles. Pedestrian access to the main entrance of the B&Q store is from Cricklewood Lane is via an existing flight of stairs and ramp on Cricklewood Green. The existing ramp, however, does not meet standards with regards to universal access and would need to be reconfigured in order to do so.

Vehicular access

The existing B&Q car park has two vehicle accesses. The first is at the south-east corner on Cricklewood Lane, in the form of a limited-movements priority junction (no right-turn out); the second is towards the north off Depot Approach. There is also a service access to the B&Q yard off Depot Approach to the west of the Site. Since The existing route through the B&Q car park is currently used as a 'rat-run' by drivers seeking to avoid the Cricklewood Lane / Cricklewood

Broadway junction. This rat-run of vehicles through the Site however only exasperates congestions at Cricklewood Lane closer to the train station. The Proposed Development provides an opportunity to actively discourage, or remove altogether, this type of vehicular traffic through the Site. The removal of the existing Cricklewood Lane access/egress would improve highway safety and provide an improved environment for pedestrian and cycle movement.

- Site
- Primary routes
- Secondary routes
- - - Railway line
- - - Disused branch of railway line
- - - Existing route through carpark
- ▲ Existing entrance to site



Map illustrating movement and access routes in and around the Site



Pedestrian access from Cricklewood Lane / Cricklewood Green



Existing vehicle access from Cricklewood Lane



Existing service vehicle access from Depot Approach



Existing vehicle access from Depot Approach to B&Q carpark

2.11 Cricklewood heritage

Site history

In early 14th century, Cricklewood began as a small rural settlement along Edgware Road, originally a Roman road which was later called Watling Street.

With the arrival of the surface and underground railways in nearby Willesden Green, urbanisation began around the 1870s. Cricklewood Station, previously Childs Hill and Cricklewood Station, was opened in 1868.

By the 1890s, houses and shops had been built along Cricklewood Lane and Cricklewood Broadway where the town centre of Cricklewood started to emerge by 1900.

Listed buildings

No properties on the Site are statutorily or locally listed.

The Crown Public House

Located to the south of the Site on Cricklewood Broadway, the Grade II Listed and "Jacobean" public house, dated 1900, is the most prominent historic building within the Site's surrounding context.

Conservation areas

There are two Conservation Areas in Cricklewood, the Mapesbury Estate and the Cricklewood Railway Terraces.

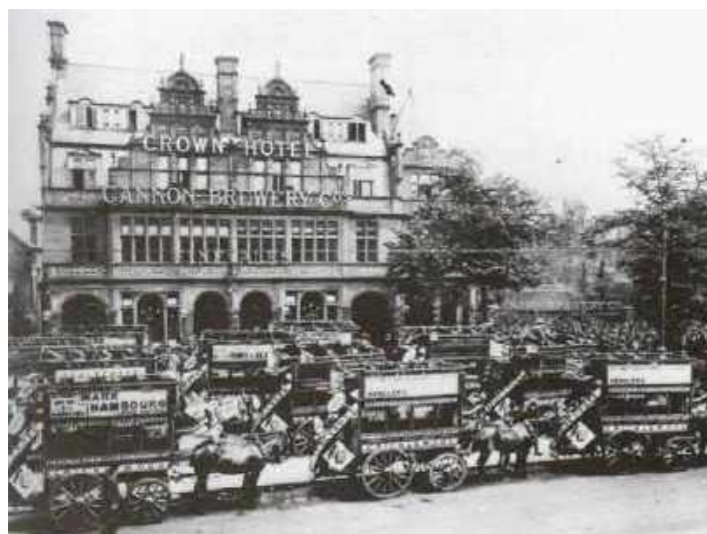
Following the Midland Railway's relocation of its locomotive works from Kentish Town to the Brent Sidings, the Cricklewood Railway Terraces were built in 1881.

Given the proximity to the Cricklewood Railway Terraces Conservation Area and the scale and character of the Proposed Development, the approach to height has been carefully considered with views from the conservation area tested and discussed as part of the consultation process with LBB.



- Site
- Cricklewood Railway Terraces Conservation Area
- Grade II listed
- Locally listed

Map illustrating the conservation areas and listed buildings within close proximity to the Site



The Crown, 1900s

(Pictures of England)



Cricklewood Lane, Queen's Hall Cinema, 1920

(Pictures of England)



Cricklewood Broadway 1912

(Pictures of England)



OS Map, 1894

2.13 Cricklewood today

Residential Cricklewood

The residential areas around Cricklewood consist of Victorian and Edwardian terraced properties, including the purpose-built workers cottages in the Cricklewood Railway Terraces Conservation Area to the north-west of the Site.

Cricklewood Town Centre and its community

Cricklewood has a strong and active community that is passionately engaged in shaping their town centre for the better. Groups such as Cricklewood Town Team and NorthwestTWO Residents' Association have been heavily involved in driving forward placemaking and public art initiatives like the artwork on and around the Cricklewood Station.

Cricklewood Town Team also play a large role in bringing residents, businesses and community groups together. The group regularly coordinate a wide range of events, from the Mobile Town Square festival to community made murals, music events, Maypole dancing, street greening and cleaning. Celebrating the local area, its history and communities is at the heart of what makes Cricklewood, Cricklewood.

In recognising the good work done to date by the existing community groups and teams, we believe our proposals will complement and build on the community's aspirations to enhance their local environment through the provision of high quality public realm and architecture, providing a focal point for the community to utilise and benefit from.



Photographs of various art installations, events and community projects in and around Cricklewood

(Photographs: Cricklewood Town Team, Thomas Ball, Mapesbury Residents Association, Andrew Moss, Alistair Lambert)

A key issue for these community events is the lack of quality public realm in the town centre.

Cricklewood High Street, 2012 - 2014, Gort Scott

In 2012, the LBB was awarded a grant from the Greater London Authority (GLA) through the Outer London Fund, to support Cricklewood to become a more accessible and thriving local town centre for all. Gort Scott, alongside the appointed team delivered shop fronts and building improvements, shop forecourts, shop window displays, site activation, new festive lighting, graphic identity and community website.

Cricklewood Mobile Town Square, 2013, Spacemakers

In response to the site-activation brief as part of the wider grant-related project, Cricklewood Town Square: a mobile public space, which could roam across the area, reclaiming forgotten patches of land, and transforming them with a programme of events into civic space for everyone to use was explored.

One of the locations this mobile town hall occupied was Cricklewood Green. The project demonstrated to a neglected community and the much-needed public space, how useful public space can be, and how vital it is in forming that community in the first place.



Photographs of the various art installations, markets, window displays that can be seen around Cricklewood

(Photographs: Gort Scott, Spacemakers)



3 Design process and consultation



Adjacent Sketch view of Cricklewood Green and the Illustrative Masterplan from Cricklewood Lane

3.1 Opportunities and constraints

Opportunities

The Site is a brownfield site, adjacent to the train station, representing pivotal opportunity to optimise housing development as well as:

- Enhance Cricklewood Green and support its role as a focal point within Cricklewood town centre;
- Improve way-finding along Cricklewood Lane towards Cricklewood Station;
- Improve pedestrian connectivity to Kara Way Playground;
- Maximise south-facing aspects of the Site with considered location of public and private amenity;
- Extend the primary retail frontages from Cricklewood Lane onto the Site;
- Maximise views towards Hampstead Heath;
- Create a landmark building improving legibility to Cricklewood Green and Station; and
- Improve passive surveillance, pedestrian safety and accessibility.

Constraints

- Noise due to proximity to the railway; and
- Limited flexibility on Depot Approach as it is the only access road to the Jewson site.

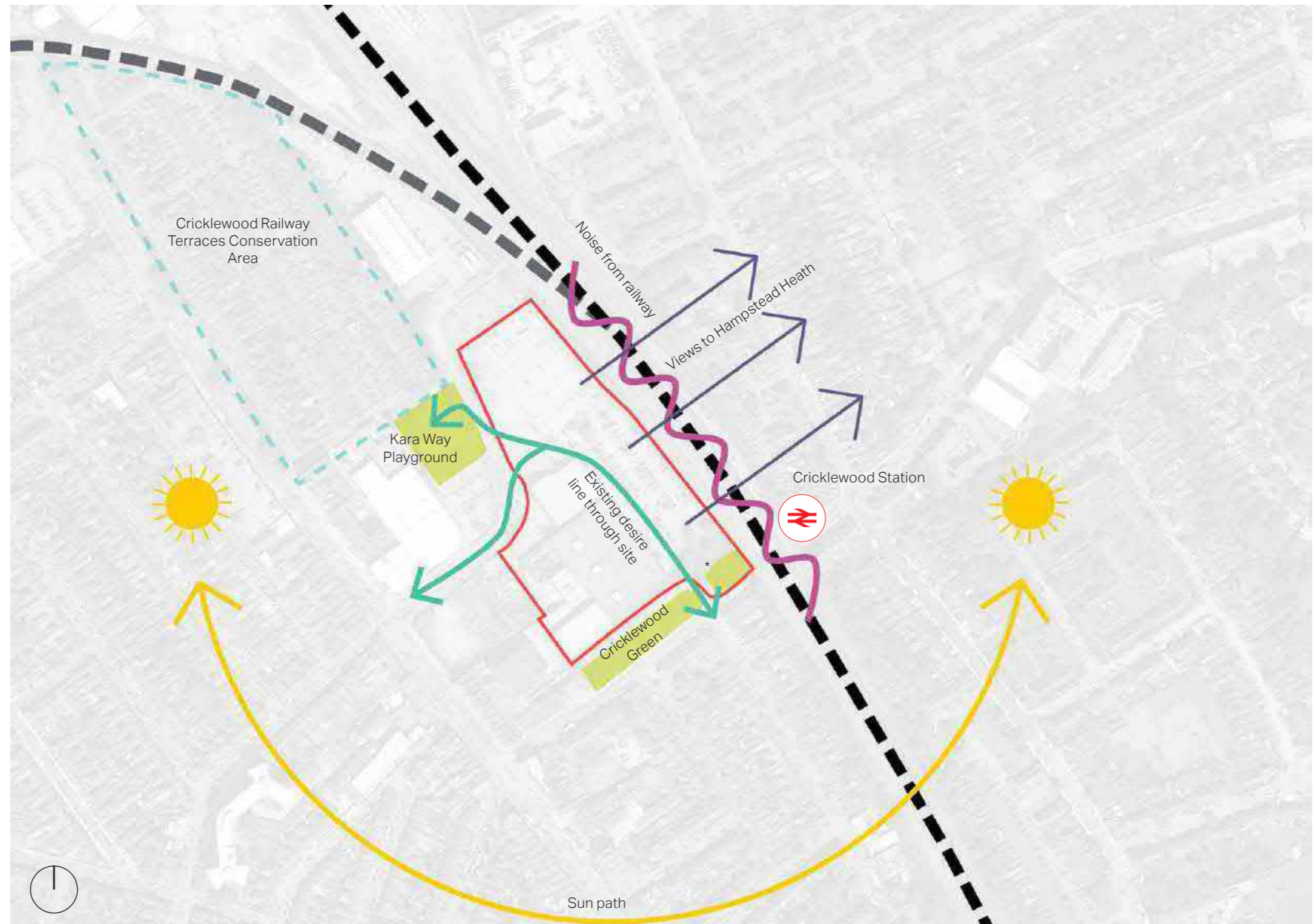
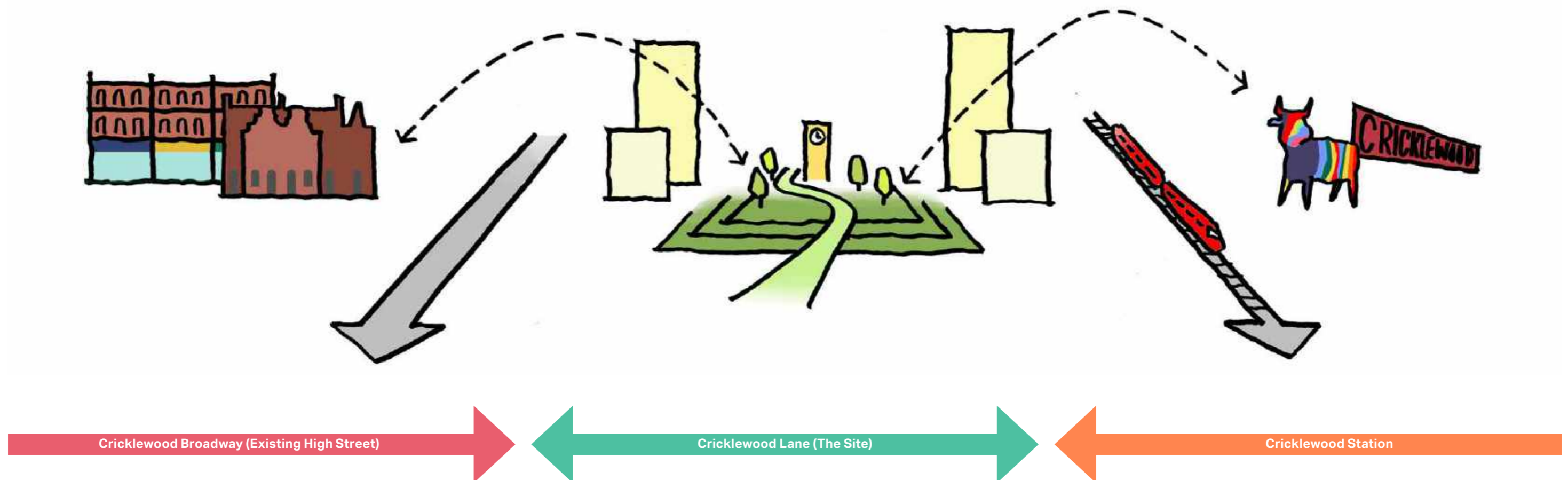


Diagram illustrating the opportunities and constraints of the Site

3.2 Vision

The vision for the Proposed Development is underpinned by core values of high-quality design and sustainability and driven by the following key objectives:

- Making optimal use of the Site’s urban characteristics, and responding to its context – Cricklewood Broadway, Depot Approach and via the Cricklewood Railway Terrace Conservation Area;
- Designing from the ground floor up, and fully considering the spatial requirements of residential and commercial operators, delivering positive socio-economic impact and a vibrant neighbourhood;
- Responding positively to the residential and commercial demand and price point of Cricklewood and supporting the growth of local businesses;
- Providing a mix of one, two- and three-bedroom homes in a range of configurations, maximising the variety of dwelling types and tenures;
- Meeting the needs and expectations of existing and emerging communities of Cricklewood, LBB, and London, and delivering a tailored affordable housing solution;
- Creating a legible and well-connected development, giving priority to pedestrians and cyclists by providing access routes that link into the town centre, train station and surrounding amenity spaces;
- Supporting a locally loved Asset of Community Value - Cricklewood Green, by enhancing public amenity space on the Site;
- Providing a proportionate level of parking, which strikes the necessary balance between prioritising the use of public transport, cycling and walking; and
- Encompassing the comprehensive redevelopment of a centrally located site, that positively responds to the local context and complements the success of existing retail and commercial businesses within Cricklewood.



3.3 Key objectives

- Create a new civic heart for Cricklewood, a destination and gathering place for local festivals and events, ensuring long term enjoyment by the local community;
- Provide a new civic space and community facilities, reflecting and building on Cricklewood local residents' civic aspirations and pride;
- Act as a catalyst for regeneration at Cricklewood, unlocking the potential of adjacent sites;
- Enhance and connect to the much-loved existing Kara Way play space and Cricklewood Green, improving both quantity and quality of public open space in Cricklewood town centre;
- Create new pedestrian links through the Site, improving site permeability to local amenities and ensuring the maximum amount of available space is given over to pedestrians;
- Improve the frontage along Cricklewood Lane, enriching the pedestrian experience towards the station and new town square;
- Contribute positively to the local townscape, adding legibility to the district centre and Cricklewood station;
- Improve the setting of the Cricklewood Railway Terrace conservation area by significant upliftment to the surrounding public realm;
- Provide affordable homes for people of Cricklewood.
- Guarantee the redevelopment of a pivotal brownfield site that lies within the Brent Cross and Cricklewood Opportunity Area by maximising higher-density development (further supported by national policy objectives) therefore relieving development pressure on green field sites;
- Create a coherent and well-connected development;
- To ease traffic and congestion by enabling locals to take advantage of the close public transport links and cycling and walking routes;
- Meet the needs of the existing and emerging communities;
- To act as a catalyst to unlock potential for future regeneration of Cricklewood; and
- Deliver a vibrant, aspirational neighbourhood that will benefit new and existing communities.



Precedent imagery capturing elements of the aspirational look and feel relating to the key objectives for the Site.

Community



Establish a civic heart to the town centre with a community focus

Public Realm



Create green, pedestrian focussed links and connections through the site

Commercial



Repair existing Cricklewood Lane commercial frontage

Housing



Deliver quality affordable housing



Create an aspirational place for new and existing residents of Cricklewood to make use of



Provide generous publicly accessible green space



Support and complement local economy, providing civic spaces that support community initiatives



Provide a mix of dwellings including family sized homes

3.4 Masterplan principles

Initial thoughts on the key concepts and guiding masterplan principles became evident following the appraisal of the Site and surrounding context as well as research into the needs of the existing community.

Two key principles emerged which remained at the centre of the development of the masterplan throughout the design process:

1 The creation of civic space

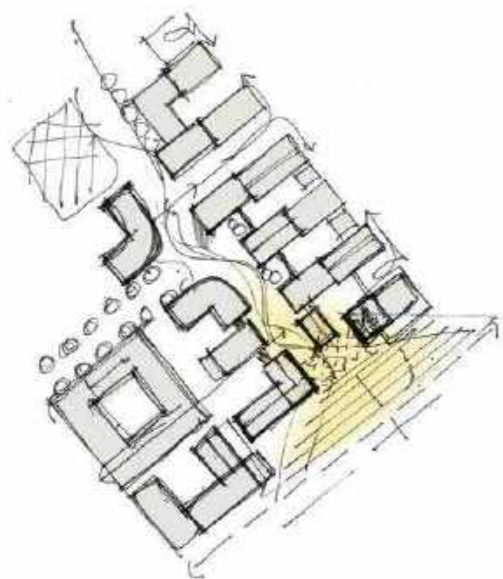
There is limited usable public open space within Cricklewood town centre and no definitive centre or 'heart' of the town. The Site offers a unique opportunity to create a new centrally located public space serving the wider Cricklewood community.

Active frontages along the perimeter of town square would further activate the public space. Reference to a clock tower could also enhance the civic qualities of this space and capture a historic reference to the Smiths clock factory that used to exist in Cricklewood, and to the decorative clock that used to exist on Anson Road.

When seen from afar the town centre location of the Site can be marked by taller residential buildings sitting on the square; contributing to the emerging identity and civic pride of Cricklewood within the wider area of North London.



Early illustrative sketch of the possible realisation of a new civic space provide with the Proposed Development



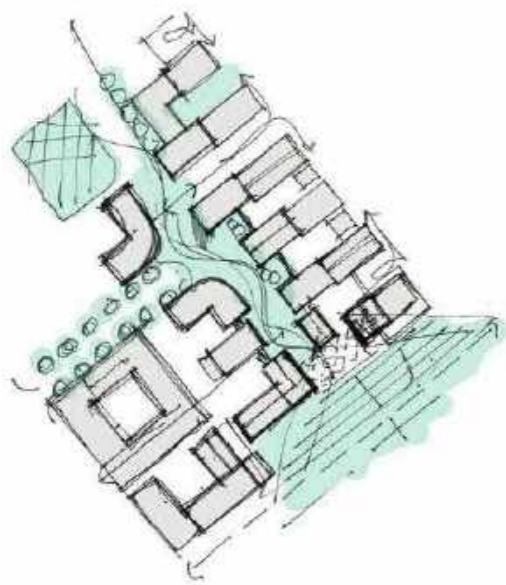
2 The provision of public open green space

Taking inspiration from the communal gardens between the Railway Terraces Conservation Area, the incorporation of a series of public open green spaces within the Proposed Development would not only give the impression of extending and connecting Cricklewood Green into the Site but also provide a green link connecting to the existing Kara Way Playground.

This series of public open spaces have the potential to form a significant piece of public realm - something that is much needed in Cricklewood town centre.

High quality landscaping provides attractive views to the ground floor homes and safe and enjoyable spaces for both residents of the Proposed Development and the wider Cricklewood community to make use of.

Passive surveillance of these space from the homes above will increase security and mitigate anti-social behaviour.



Early illustrative sketch of the possible realisation of a series of public open green spaces within the Proposed Development

3.5 Consultation and design evolution

This chapter of the Masterplan Design and Access Statement sets out the consultation and design evolution leading up to the current Masterplan submitted for approval.

LBB Consultation

A Planning Performance Agreement (PPA) was agreed between Montreaux and LBB setting out the agreed planning programme, series of meetings, workshops and planning application documents for submission.

Design evolution

Alternative massing and layout options have been tested to explore how best to achieve the requirements of the Site's allocation in the Brent Cross – Cricklewood Opportunity Area Framework and in the Cricklewood, Brent Cross and West Hendon Regeneration Area within the constraints of the existing context.

Throughout the design process there has been regular consultation with the LBB, Greater London Authority (GLA) and other statutory stakeholders. In addition, a community engagement programme has been undertaken to inform the design process, as discussed in the Statement of Community Involvement (SCI) prepared by AECOM and submitted in support of the Outline Planning Application for the Proposed Development.



Design evolution and testing of various height strategies took the form of physical models, drawings, digital testing of scenarios in VuCity, discussed and developed at regular workshops with LBB and the design team

Public Engagement

A project website has been developed to share information and updates on the proposals (www.bandqcricklewood.co.uk).

A dedicated consultation email address and phone number has been established in addition to one-to-one meetings which have been held with local resident and community groups to understand their aspirations for the Site including:

- The Railway Terraces Residents' Community Association;
- Cricklewood Business Association;
- Cricklewood Town Team;
- Cricklewood Community Forum;
- NorthwestTWO Residents' Association;
- Cricklewood Groves Residents' Association; and
- Fordwych Residents' Association

Site ward councillors have been regularly engaged via email and one-to-one meetings.

A two-day drop-in public consultation was held on Sunday 2nd and Monday 3rd February 2020 at Ashford Place which was well attended by 143 local people.

In May a project update newsletter including a summary of feedback from the drop-in public consultation was emailed to ward councillors, key community groups and residents who attended the drop-in event in February. The project website was also updated in line with this information.

2019	24 May 2019	Pre-application Workshop 01	
	25 June 2019	Pre-application Workshop 02	
	16 August 2019	Pre-application Workshop 03	
	18 September 2019	Members' Briefing	
	3 October 2019	Meeting with The Railway Terraces Residents' Community Association	
	11 November 2019	Meeting with Cricklewood Business Association	
	14 November 2019	GLA Presentation	
	12 December 2019	Pre-application Workshop 04	
	2020	8 January 2020	Meeting with Cricklewood Town Team
		8 January 2020	Meeting with Cricklewood Community Forum
10 January 2020		Meeting with NorthwestTWO Residents' Association	
21 January 2020		Meeting with Cricklewood Groves Residents' Association	
23 January 2020		Meeting with Fordwych Residents' Association	
29 January 2020		Meeting with The Railway Terraces Residents' Community Association	
2 – 3 February 2020		Public Consultation	
22 June 2020		Meeting with LBB	
13 July 2020		Meeting with Councillor Peter Zinkin, Councillor Shimon Ryde, Service Director of Planning and Building Control at LBB and Planning Manager at LBB	
17 July 2020		Meeting with NorthwestTWO Residents' Association, Councillor Anne Clarke, Councillor Lia Colacicco and Councillor Arjun Mittra on behalf of the GLA Assembly Member for Camden and Barnet.	
31 July 2020	Outline Planning Application Submission		

Pre-application Workshop 01

24th May 2019

The first of four formal pre-application meetings were held with the LBB between May 2019 and December 2019 was held in May 2019 - as agreed under a Planning Performance Agreement (PPA) between the Applicant and the LBB.

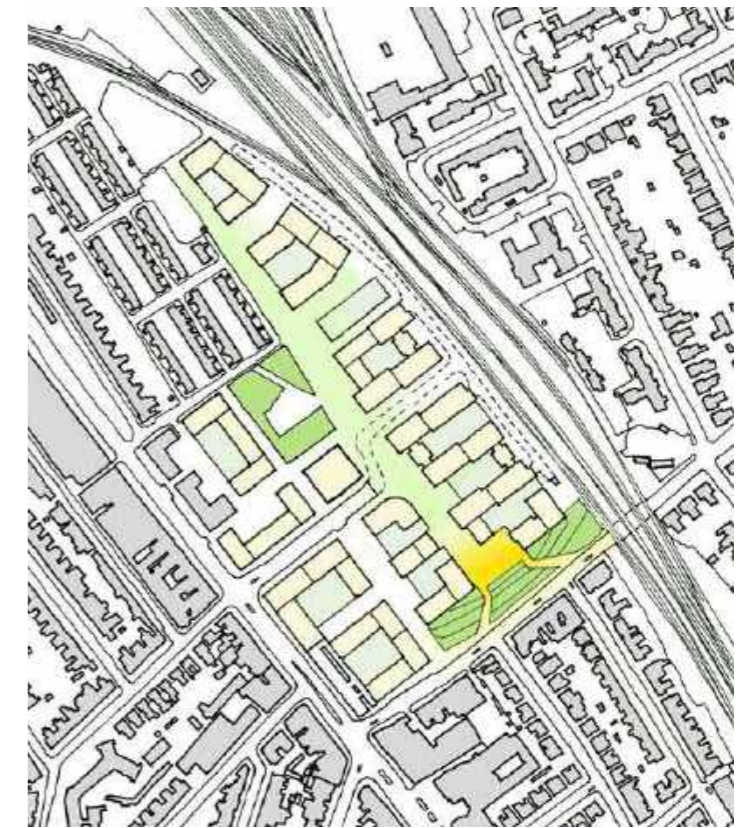
This initial meeting provided the opportunity for design team introductions and discussions around initial thoughts on the following:

- Aspirations and Key Objectives;
- Site Context;
- Opportunities and Constraints;
- Placemaking – Vision and wider masterplan; and
- Initial Masterplan Proposal.

Key outcomes

- Recognition that this is a transition site and creating a sense of arrival is key with regards to the pedestrian experience to and from the train station;
- Improvements to the train station likely required – potential lighting scheme under tunnel (secure by way of S.106);
- Technical practicalities to be mindful of include adjacency of the railway and A5;
- High level exercise in testing a wider masterplan to be carried out with LBB to ensure that the Proposed Development doesn't preclude future development on adjacent sites;
- Engagement and messaging around Cricklewood Green is key, acknowledging that this is an important space for local residents;
- LBB encourage early engagement with the Railway Terrace Conservation Area action group;
- Step down in height towards the north broadly welcomed, however alternative locations of height to be developed further and informed by testing of key townscape views (agreed with LBB);

- Demonstration of appropriate scale of public open green spaces and town square to be reviewed to assist LBB gain a better understanding of scale;
- Commercial uses should support the existing commercial offerings of the town centre and not compete or detract from them;
- Survey of the existing B&Q car park is key to better understand existing use and trips;
- LBB highlighted a closed underpass connecting the Site to the train station. Suggested that a portion of land safeguarded for any future access requirements; and
- Travel Plan to account for new TfL guidance and requirements regarding healthy streets.



Early illustrative sketches of emerging proposals and aspirations for public realm

Pre-application Workshop 02

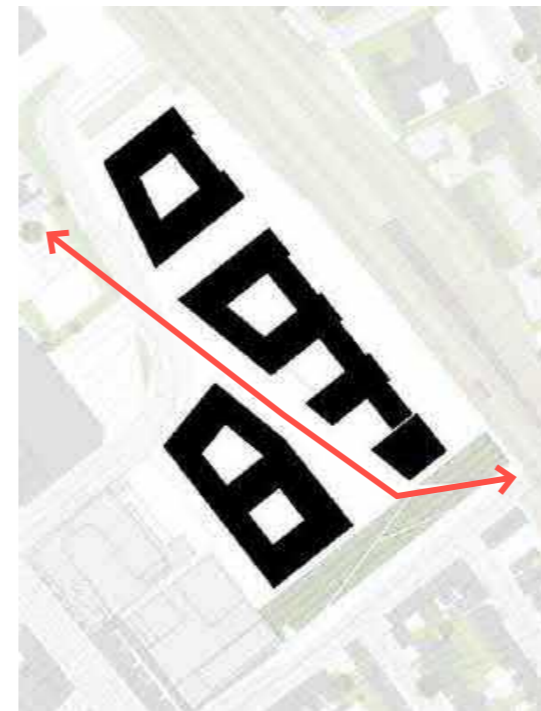
24th June 2019

The second pre-application meeting with LBB acted as a workshop to review and compare alternative height strategies via a physical model as well as comparative townscape keys (as agreed with LBB following a site visit on the 18th of June 2019).

Key outcomes

Landscape vision and concept

- LBB expressed aspirations for a green space on the Site adjacent to and supporting Kara Way playground (aspirations for shared surface and traffic calming measures to be introduced at this point on Depot Approach);
- LBB feedback regarding landscape vision and concept was very positive, LBB feel that it will provide a high-quality space; and
- LBB agreed with the design approach of the ground floor public realm to guiding the Masterplan as the narrative surrounding the creation of open public space provides a very strong basis for moving forward.



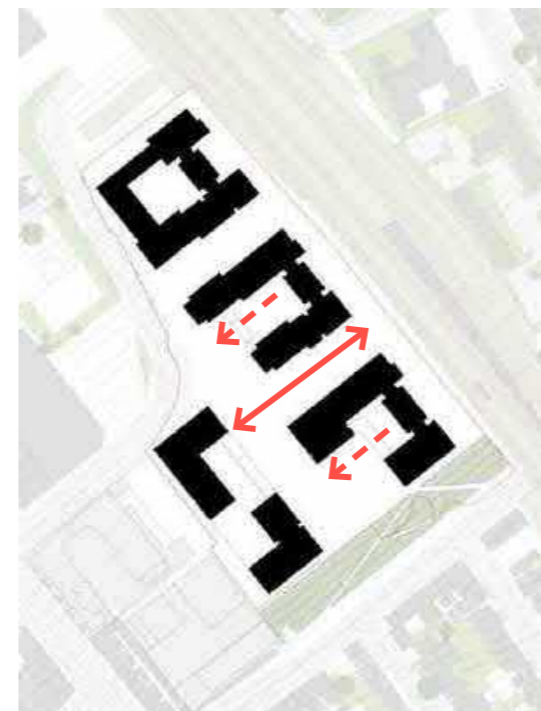
- Build footprints responding to existing and possible future context;
- Strong connection to and from the train station.



- Establish town square space;
- Linear route through site;
- E-shaped building to respond to sun path.



- Increased variation along edge of public realm;
- Possible community facilities spilling out onto town square.



- Increase public green space by splitting development parcels;
- Open up residents' podiums to the south increasing sunlight/daylight.



- Express marker building at town square;
- Further opening up of podium amenity spaces.



- Rotate key building to open up views from station approach (a result of VuCity test views).

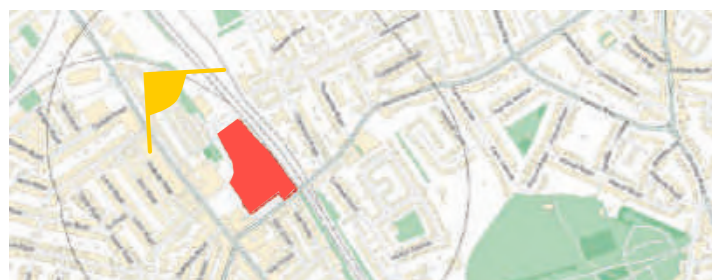
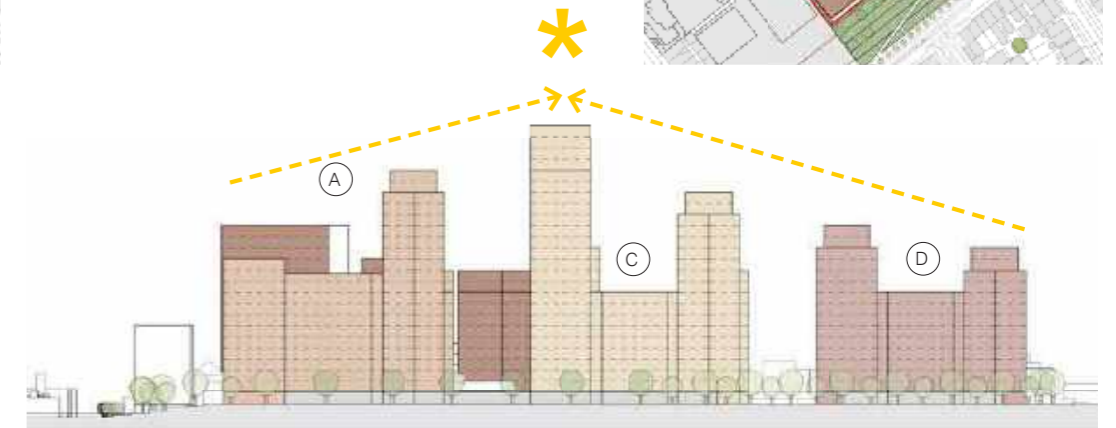
Height strategy

- LBB suggestion to explore a slight increase in height to the tallest element of Building A to enhance the legibility of the Proposed Development and accentuating the step in height from the surrounding buildings;
- Possible reduction in height of Building D closest to the Railway Terraces to be further explored; and
- Building B massing to be further developed to reduce the 'wide' read of the Cricklewood Lane elevation and when viewed from the station bridge underpass.

Massing option 1



Massing option 2



View from Cricklewood Railway Terraces - Massing Option 1 (VuCity)

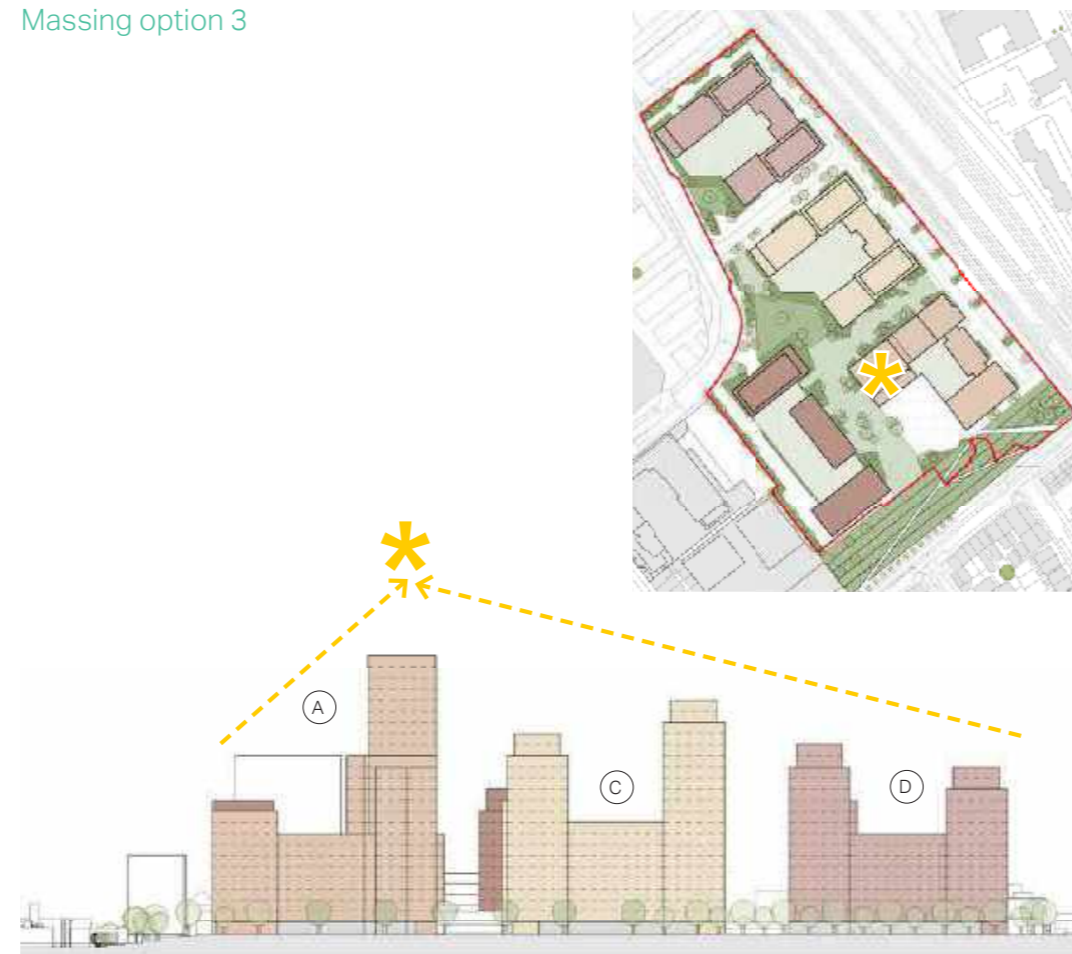


View from Cricklewood Railway Terraces - Massing Option 2 (VuCity)

Townscape views and massing options

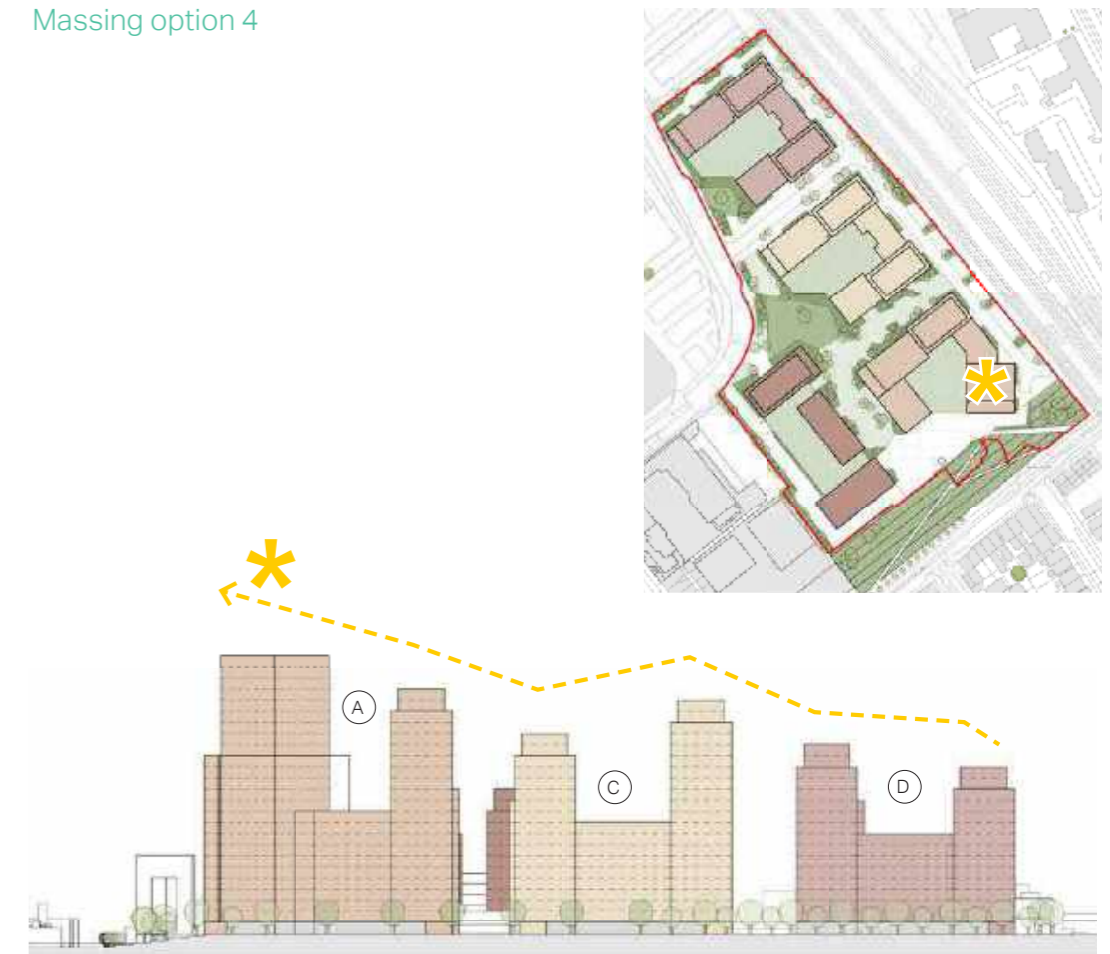
- Generally positive feedback, progress of design development welcomed by LBB. LBB expressed preference for Massing Option 4 (allows for future-proofing of potential subway link), comfortable with height along the railway, felt tallest building could potentially take slightly more height if beneficial to clarity/legibility of the Proposed Development in accentuating the step in height;
- Design team to continue to develop Massing Option 4, with variations looking at accentuating the step in height to the tallest building to be tested;
- Massing of buildings around the square which present 'wide' in views from station bridge underpass and Elm Road and form focal point to these views to be developed and impact of step massing to be explored;
- Justification of height strategy of Building D nearest to the Railway Terraces to be further explored through townscape views and wider masterplan consideration;
- Quality of pedestrian experience and landscaped public realm to remain a focus of design development; and
- 1 - 13 Cricklewood Lane revised scheme of 9 storeys to be represented in draft townscape views and on physical model.

Massing option 3



View from Cricklewood Railway Terraces - Massing Option 3 (VuCity)

Massing option 4



View from Cricklewood Railway Terraces - Massing Option 4 (VuCity)

Pre-application Workshop 03

16th August 2019

The Proposed Development presented and discussed at Pre-application Workshop 03 responded to key headlines and outcomes from previous discussions with LBB namely:

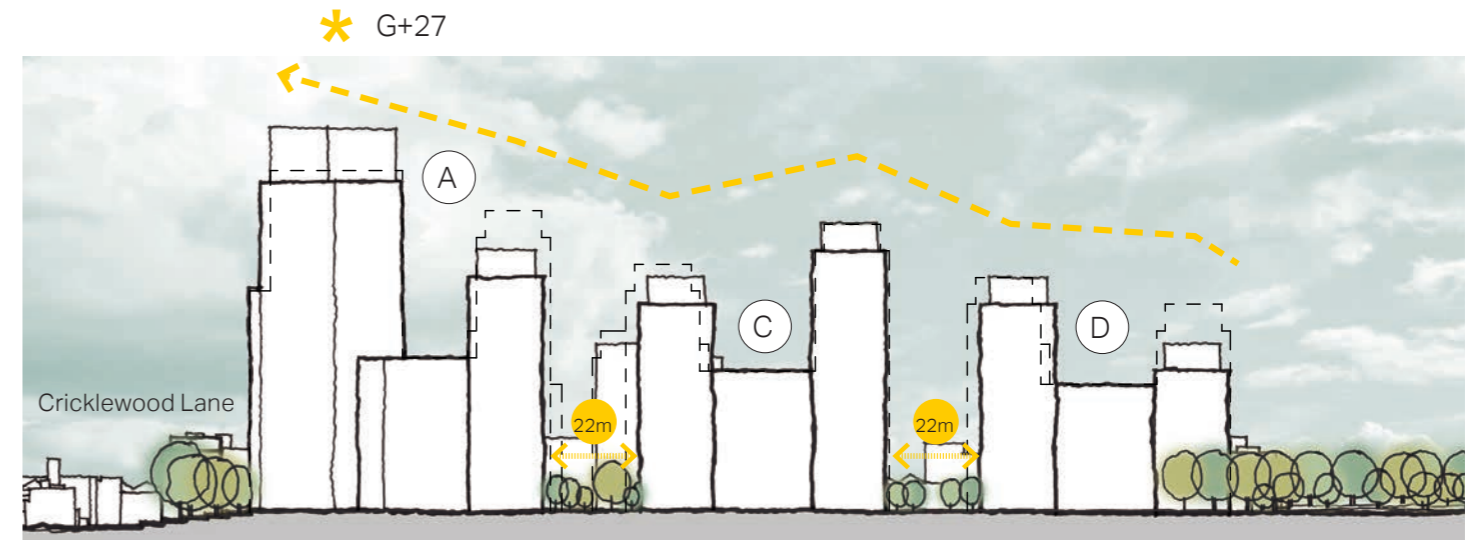
- Building D footprint revised to increase area of public realm adjacent to Kara Way Playground;
- Massing adjusted to increase the distance between Buildings A and C, and Buildings C and D, improving levels of daylight/sunlight.
- Heights reduced across the Site, particularly towards the north; and
- Slight increase in height explored at Building A.

Key outcomes

- Re-orientation of development Building B next to Cricklewood Lane welcomed, enhancing arrival to the town centre upon exiting the underpass;
- LBB voiced that 28 storeys is likely to be a key consideration with Members recommending that this is reviewed, emphasising that the wider planning benefits of the Proposed Development are key to building the planning narrative and justifying scale and massing;
- LBB noted that views from the Cricklewood Railway Terraces' allotments and Crown Pub should be included in future townscape views;
- LBB recommended a consolidated area of green space be explored adjacent to the Railway Terraces and Kara Way Play area (which is currently overused) increasing play provision;
- LBB raised issues with anti-social behaviour near to Kara Way play area - a key concern for residents in the Railway Terraces;
- LBB noted that the B&Q site redevelopment should facilitate LBB's future aspiration to redirect Depot Road. An aspirational diagram/plan was recommended to show how this could work in practice.



Visual connection to podium landscape



Massing 5 - Sketch elevation from Cricklewood rail platform showing variation in height along the railside edge of the Site



Massing 5 - Sketch aerial view showing design development and height adjustments made following Pre-application Workshop 03

GLA Presentation

13th November 2019

Productive meeting was held with the GLA and LBB.

Key outcomes

Principle of the Proposed Development

Supported, acknowledging that Site is exact place where GLA would seek to push housing (including affordable);

Design

Acknowledge transitional site with quantum of development and general layout supported. Recommended that the proposed commercial uses are supported by local needs assessment, but no concerns with applying for current flexible range of uses (B1, D1, D2);

Open space strategy

Suggested that this is reviewed to make simpler and ensure corresponds with phasing, as well as overall masterplan;

Highways

TfL confirmed happy with 10% car parking provision;

Townscape

Detailed townscape analysis to be carried out in conjunction with review of daylight, sunlight and overshadowing.



ExA sketch view of Woodway - green link through the Site



ExA sketch view of Cricklewood Green and the new town square / civic space



ExA sketch view of increased playspace adjacent to Kara Way playground

Pre-application Workshop 04

12th December 2019

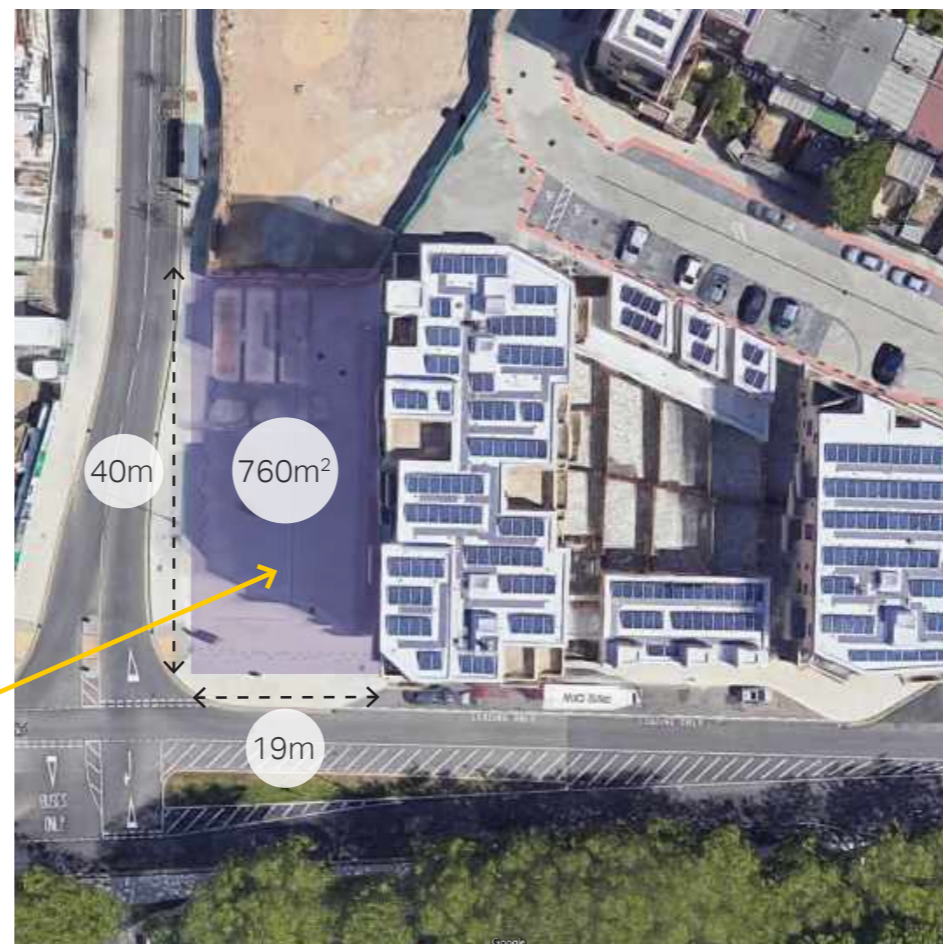
The Proposed Development presented incorporated:

- Reduction in height of the tallest building element from 28 to 25 storeys;
- The tapering down of height towards the north rather than the initially suggested variation in height along the railway;
- Increase in size of playspace and public realm adjacent to Kara Way playground; and
- The above amendments resulting in a reduction in the number of anticipated homes from 1250 to 1100.

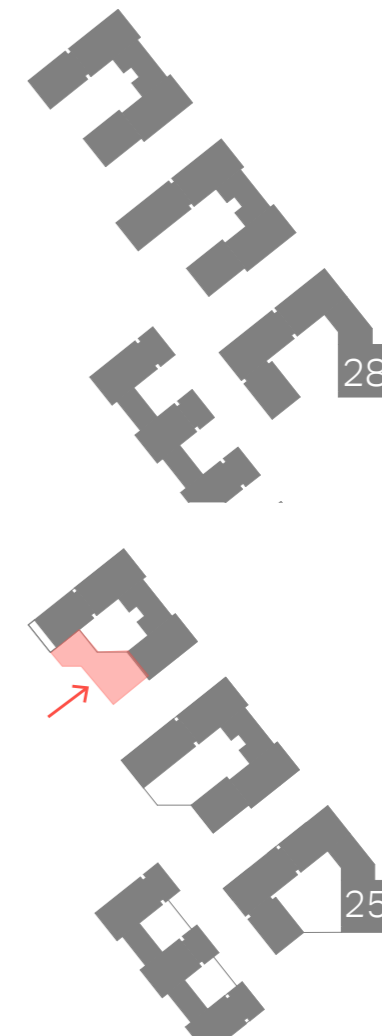
Key outcomes

- LBB confirmed they are comfortable testing the current height as shown within the GLA presentation (25 storey maximum);
- The principle of comprehensive residential-led redevelopment of the Site is in place – supported by Cricklewood Opportunity Area;
- The draft Development Phasing Plan was presented to the team – which showed the Site coming forward in 3 Development Phases. Buildings A & B coming forward in Development Phase 1, Building C in Phase 2 and Building D to the north in Phase 3. LBB signed off the proposed approach;

- Illustrative Masterplan - Flexibility is sought regarding the commercial space strategy;
- LBB commented that this looked high, and that play space needs to be fully integrated into the landscaping proposed and not detract from the linked route through the Site;
- Need to safeguard route to the Jewson site, future-proofing the Site as part of the wider masterplan; and
- LBB agree with the proposed residential car parking ratio of 10%.



Comparative representation of key public open spaces. The public space outside LBB new offices is used as reference for the proposed town square space.



Pre Application Workshop 03

Tallest building 28 storeys;
Units No. 1250

Pre Application Workshop 04

Tallest building 25 storeys;
Playspace increased to support Kara Way playground.
Units No. 1100

**Public Consultation
2nd and 3rd February 2020**

A drop-in public exhibition was held on Sunday 2 February 2020 11am - 3pm and Monday 3 February 2020 3.30pm - 7.30pm at Ashford Place, a community centre close to the Site.

A total of 143 people attended – 91 on the Sunday and 52 on the Monday.

The event was advertised with 5,298 A6 flyers delivered door-to-door to addresses in Barnet, Brent and Camden. The flyers were also left at key community venues, and a digital version was emailed directly to key community/resident stakeholder groups who then circulated it to their networks and posted it on community websites and their social media accounts.

A feedback form was provided for people to complete at the event or take away and email or post (free of charge) back to Iceni Engagement. Team members at the exhibition also made notes of the conversations they had with attendees.

Feedback on the Proposed Development was generally constructive. Attendees understood and welcomed the community benefits including the public space, landscaped areas and enhancements to Cricklewood Green.

However, there was a lot of concern regarding the height of the Proposed Buildings, particularly the 25-storey element of Building A. Other frequently raised concerns included the impact of the proposals on existing local congestion problems. A considerable number of those who attended were also concerned about the impact that the increase in population would have on the local social infrastructure.

The following is a list of common themes and impressions from the feedback form responses and conversations at the event:

- Concerns about the height of the proposed buildings, particularly the 25-storey building;
- Positive feedback on the proposed public space and landscaped areas;
- Support for enhancements to Cricklewood Green;
- Positive feedback on the potential community uses for the new town square;
- The lack of existing green spaces locally;
- Support for the new playground;
- Support for keeping and increasing the 'Cricklewood Sheep' public artwork;
- Consideration of the infrastructure and flexibility of the proposed public space;
- Whilst most were pleased with the proposed 'car free' development, others felt more parking spaces should be provided;
- The closure of B&Q which was considered a useful shop for residents;
- The impact of construction on congestion;
- The impact of new residents on local social infrastructure;
- Maintaining the character of Cricklewood;
- The need for coordination between developments to ensure consistent character of the built environment;
- Those who talked about building materials expressed a preference towards brick and references to the local built character;
- The wider context of development locally and the perception that it is over-development;
- A need for public toilets near to the new playground and public space;
- Security and anti-social behaviour;
- The construction timeline;
- Construction impacts on immediate neighbours;
- The protection of pedestrians during construction;
- A need to consider Cricklewood's homeless and traveller communities;
- The opportunity to retain sufficient land for a second entrance to Cricklewood station should this become a priority for Network Rail;
- Encouraging wildlife and biodiversity;
- Sustainability of the proposed development; and
- Improvements to the local transport network.



Copies of the presentation boards from the event can be found at www.bandqcricklewood.co.uk



4 Comprehensive masterplan



Adjacent Sketch view of Wood Way, the central green route through the Illustrative Masterplan

4.1 Approach

The Masterplan Design and Access Statement seeks to establish a vision and framework for development across the Site.

This document outlines high level design principles through the use of a strategic design framework, while standards for future RMAs are set out in the Design Guidelines.

Together, these documents provide guidance to achieve a comprehensive masterplan. Future RMAs should bring with them their own rigour and style, and in doing so, add positively to the richness of surrounding context and character of Cricklewood.

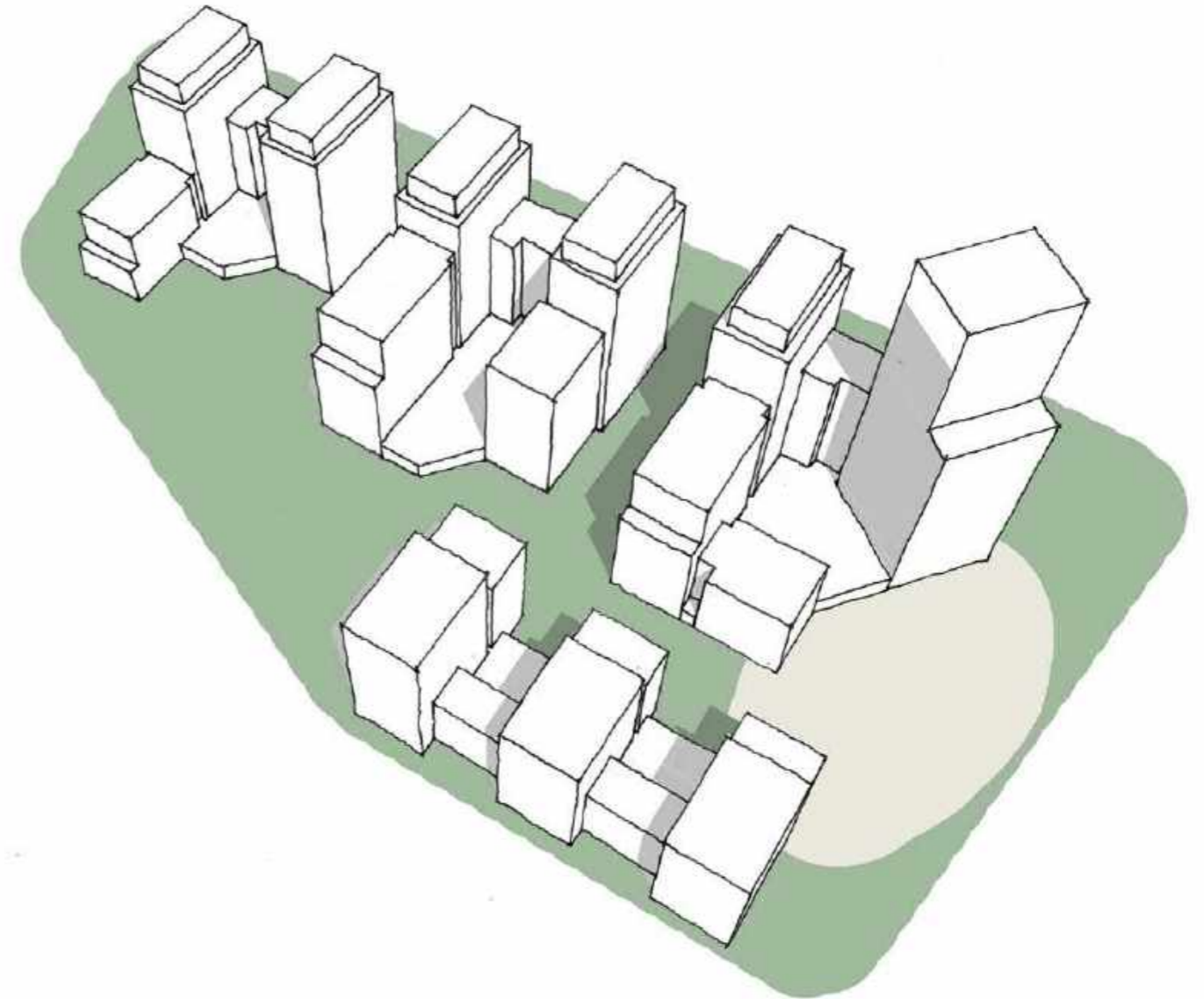
The Proposed Development seeks to remodel the land at Cricklewood Lane, closing the existing egress/ingress point to the Site in order to support the aspirations for the Site to be a successful example of placemaking with high-quality public realm, and an enhanced commercial offer which promotes a night-time economy that make it stand out as a destination and focus within Barnet.

The key to that is transforming Cricklewood town centre into a thriving place where more residents want to live and spend leisure time. This is to be supported by a residential-led mixed use development which meets local housing needs and promotes a stronger local economy and sense of community.

4.2 Strategic design framework

This strategic design framework addresses:

- Development Parcels**
 The layout of residential buildings within the Site is pre-determined in relation to anticipated locations of public open spaces. This strategy has been developed in parallel with the Illustrative Masterplan landscape.
- Height strategy**
 A coordinated height strategy across the Site, responding to the surrounding context, key views as well as the Site's opportunities and constraints.
- Site wide circulation**
 Movement strategies providing an access framework for future RMAs to work within, ensuring a considered approach to access is taken.
- Uses**
 General locations and broad quantum of space for residential and flexible commercial use is indicated as part of the comprehensive masterplan to ensure public open spaces benefit from active frontages and that the privacy of ground floor private residents' amenity spaces is safeguarded.



4.3 Development parcels

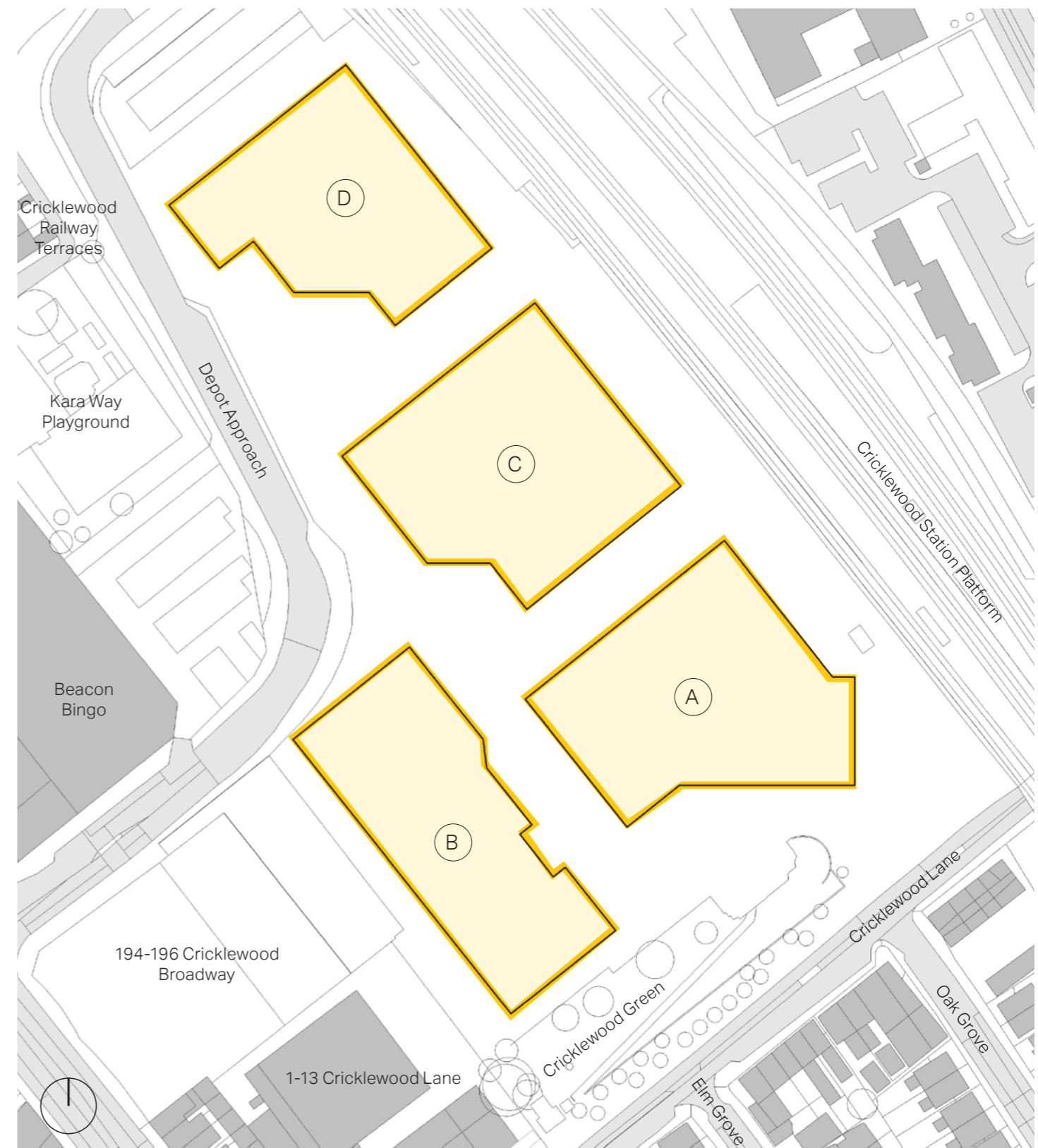
The design development described in the previous chapter of this Masterplan Design and Access Statement has led to the current masterplan.

The application includes Development Parcels A-D as defined on the Parameter Plans and illustrated adjacent. These are individual buildings across the Site.

Certain high level design characteristics and principles (tested in the Illustrative Masterplan) tie each of the Development Parcels into the overall masterplan;

- **Development Parcels A and B**, fronting onto Cricklewood Green and the new 'town square' have the potential to provide the majority of the flexible commercial offer at ground floor as active frontages in this area are encouraged while upper floors would be entirely residential;
- **Development Parcels C and D**, envisaged as perimeter buildings, albeit open to the south-west for improved daylight/sunlight. Predominantly residential in use, with central garden courtyards at upper podium level, providing communal amenity space for the residents of each building;
- **Development Parcel A**, housing the tallest building element within the masterplan, is seen a marker building identifying the new 'town square' and centre of Cricklewood.

Tying together the Development Parcels, the masterplan provides opportunity for generous landscaped routes and public open spaces across the Site.



Development Parcels within the masterplan

4.4 Height strategy

Wider context

As outlined in Chapter 2, it is recognised in the LBB's Core Strategy (September 2012) that the majority of the approved schemes within this area will involve tall buildings.

As the Site is within the Brent Cross Cricklewood Regeneration Area, proposed tall buildings on the Site should comply with the urban design principles established in the Brent Cross Cricklewood Development Framework; including:

- *Taller residential buildings will be sensitively located;*
- *Taller buildings within the higher density residential areas will not form the dominant residential typology; and*
- *Residential buildings above 15 storeys should be located along the line of the mainline railway.*

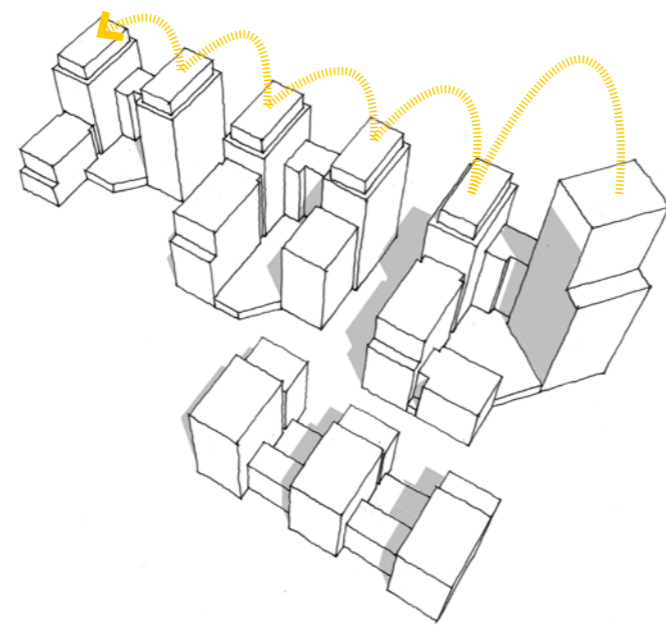
The Brent Cross Cricklewood masterplan maximises the use of the Site in accordance with the London Plan and will result in an urban form and density that is different from the surrounding area of Barnet.

Existing tall buildings are strategically located around public transport nodes and key employment and retail destinations.

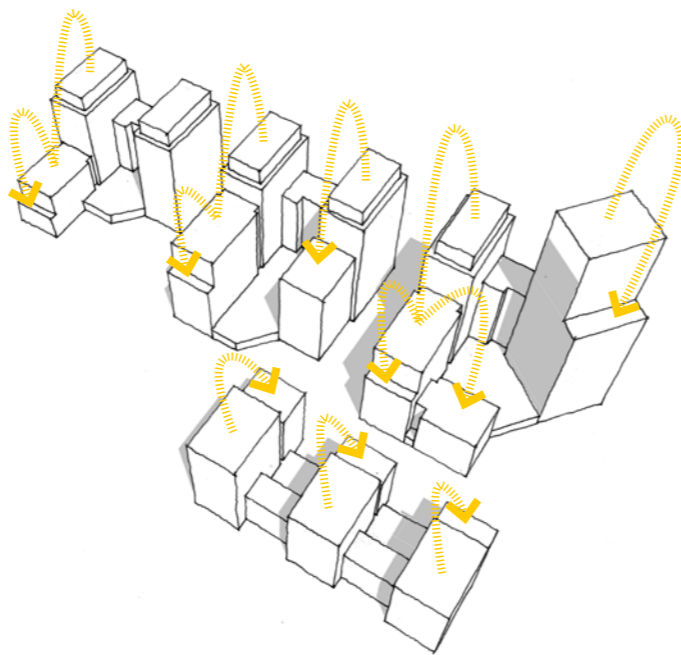
The strategy for the location of tall buildings on the Site has been informed by the policy position and site characteristics, placing the tallest elements of the Proposed Development closest to the railway lines and taking into account local viewing corridors.

The Site

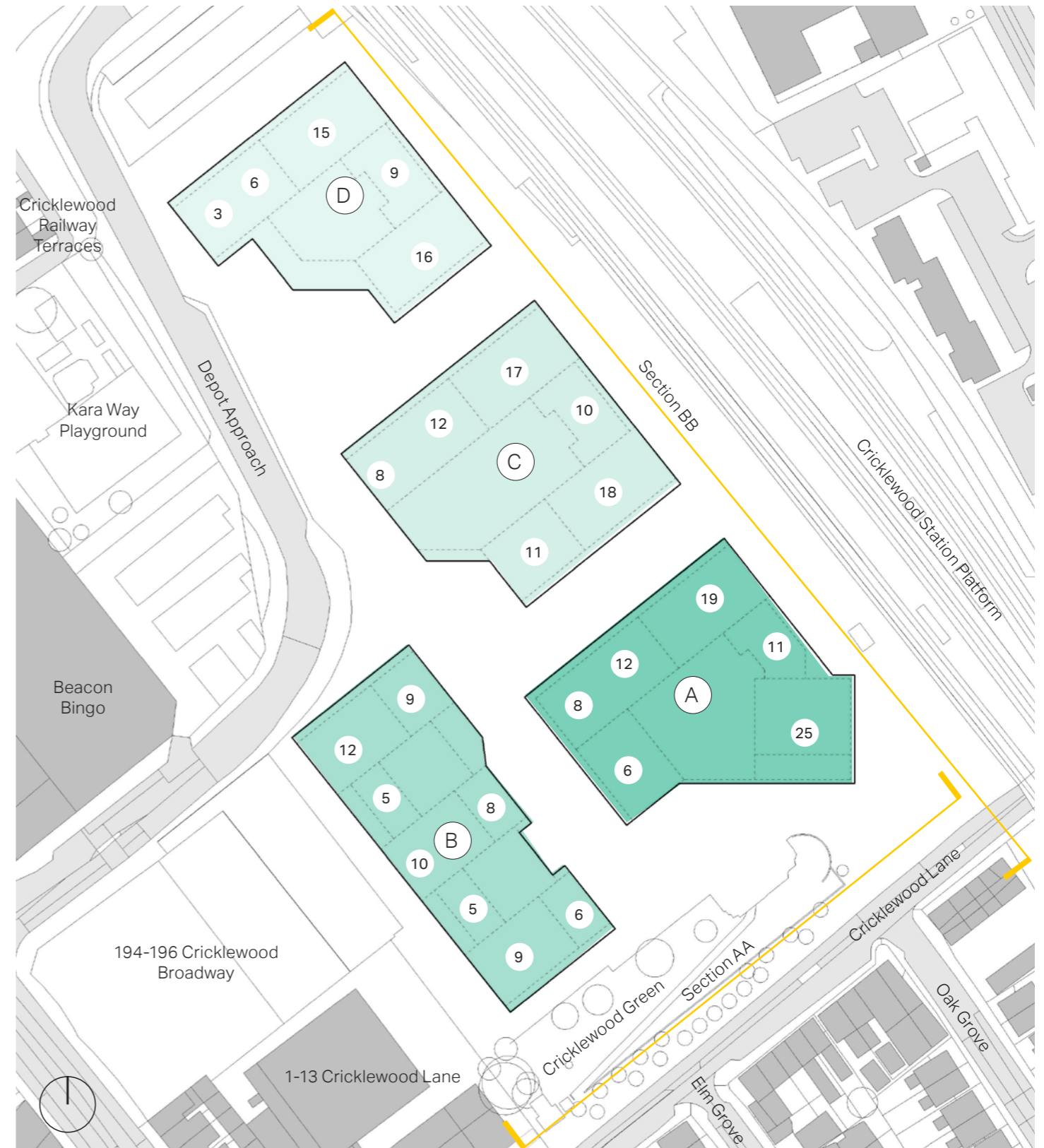
Options for the location of the tallest elements of the Proposed Development, and the composition of the buildings in the stepped manner, were tested as part of the pre-application process. It was the conclusion of this exercise that by locating the tallest building on the corner of the Site to the south not only resulted in the most successful composition but was found to be less impactful visually when tested from areas of significant open space and within the setting of designated heritage assets to the north and west of the Site.



Heights decrease towards Railway Cottages



Heights decrease towards central green route and public realm



Development heights tested in the Illustrative Masterplan to establish maximum heights for the respective development parcels

Lower, connecting, arms between each of the taller building elements provide visual relief in height to the overall massing when viewed from either the south west (the proposed Wood Way or Depot Approach) or when experienced from areas to the north, such as the train platform. This approach also improves levels of sunlight and daylight and makes the most of the long distance views that are available.

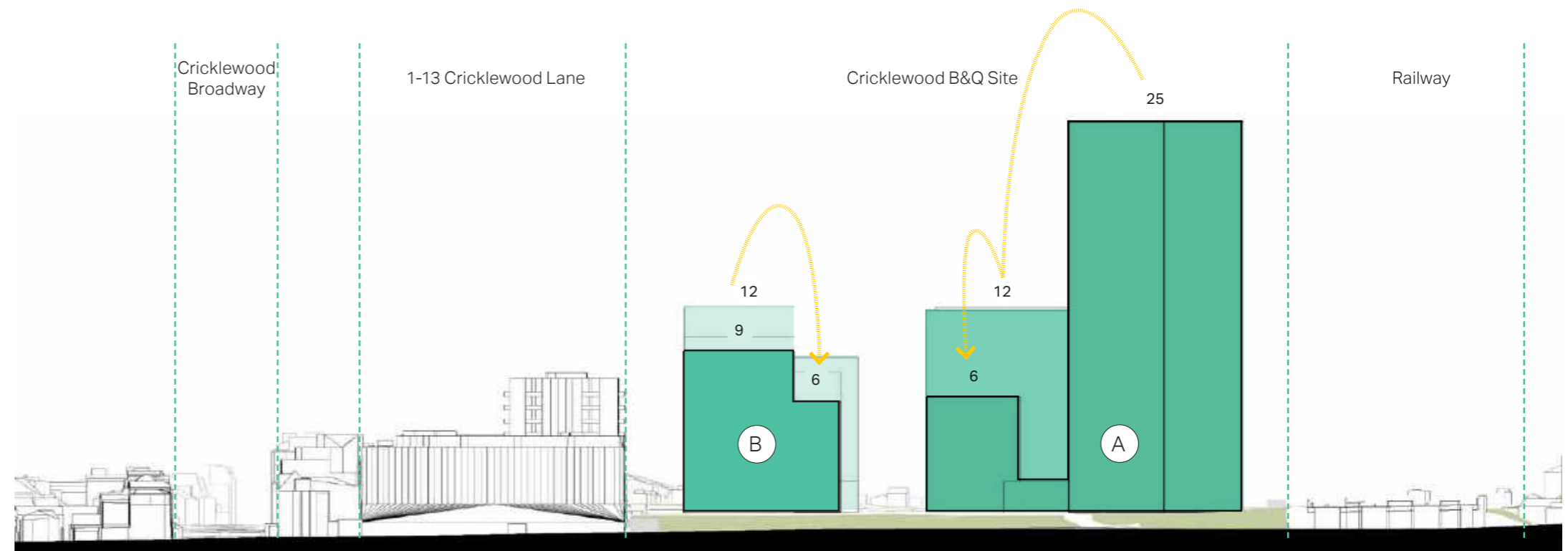
The taller building elements along the railside range in height from 15 (Building D) to 25 storeys (Building A). Height decreases towards Cricklewood Railway Terraces while maximum height is located nearer to Cricklewood train station and the new 'town square'. Heights along the railside gradually increase towards Building A.

At 25 storeys in height, Building A functions as a marker to Cricklewood train station and the new town square. The height of the Building A has been capped by considerations associated with key townscape views from surrounding areas identified and rigorously tested as part of the pre-application process.

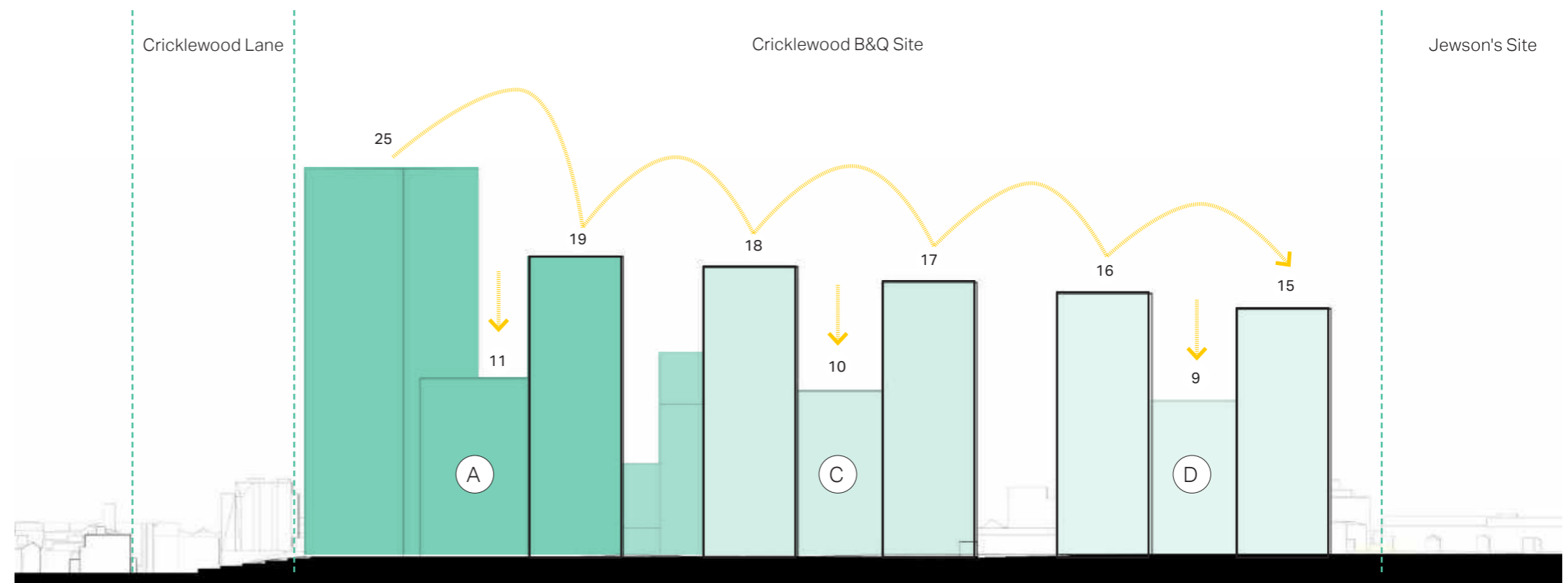
The 'shoulder' elements of buildings A, C and D, fronting the Wood Way public realm, provide a transitional scale between the taller building elements along the railside, creating a more intimate, human scale along the proposed internal Wood Way.

Building D, nearest to the Cricklewood Railway Terraces, steps down to 3 storeys - the lowest element within the Proposed Development. Careful consideration has been given to the heights of Building D in particular given its proximity to the Cricklewood Railway Terraces which has included the testing of impact on key views from this area. It was agreed with LBB that the heights proposed for Building D should also take heed not to blight meaningful future development of the adjacent Jewson site.

The distribution of height throughout the masterplan was tested as part of different compositions using visual impact analysis in VuCity with the Architects, and subsequently with visualisation consultants at Cityscape.



Section AA - Proposed Development site section along Cricklewood Lane



Section BB - Proposed Development site section along railway

4.5 Site wide circulation

As indicated by the diagram opposite, the Proposed Development would remove the existing entrance/exit point on Cricklewood Lane. The Proposed Development will be served by a single vehicular access point into the Site, via Depot Approach, appropriate for the forecast low levels of vehicle traffic associated with the Proposed Development. The removal of the existing Cricklewood Lane access will remove the 'rat run' used by vehicles during peak times through the Site, to the benefit of highway safety and pedestrian amenity.

Pedestrians

The proposed masterplan provides a central north-south connection through the Site, dedicated to the use of pedestrians (and cyclists) with the exception of the occasional operational or emergency vehicle movement as noted below. The route will be located within an area of high-quality public realm and therefore designed in line with the Healthy Streets principles. Pedestrians can make use of all ground floor public open space provided on the Site, with the anticipated dominant route being through the centre of the Site linking Kara Way Playground and Cricklewood Green.

Cycles

Cyclists will be able to pass through the Site in a north-south (and vice versa) direction, as indicated by the diagram opposite; it is anticipated that the provision of new cycle route through the Site will connect into and enhance the existing network of cycle routes in the surrounding context.

Emergency vehicles

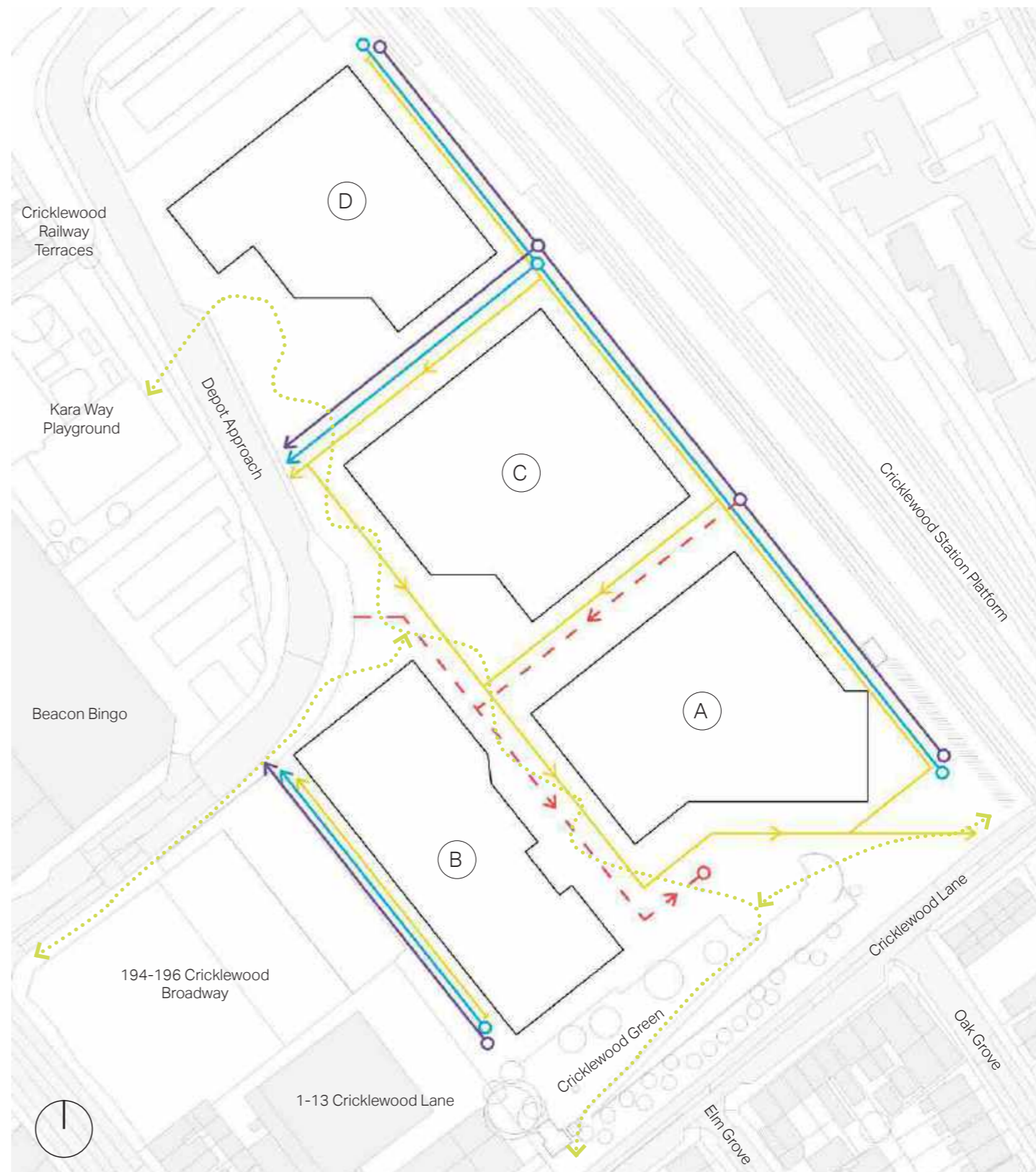
Access for emergency vehicles is as indicated on the diagram opposite. All the strategic pedestrian and cycle routes through the Site will be designed to accommodate emergency vehicles in order that all areas of the Site, including the 'town square' space, are fully accessible to emergency vehicles.

Non-service vehicles

The proposed internal road running parallel to the railway provides an access route for residents' cars and service vehicles. All internal roads will be designed as low-speed environments using materials to achieve balanced streets. Once on-site, vehicle movements will be restricted with access points into below-podium car parking located along the railside, with the exception of Building B.

Service vehicles

Larger commercial vehicles servicing the flexible commercial spaces will use the railside road to the rear of the buildings; designed for the use of 10m rigid lorries (maximum) and refuse vehicles. Refuse Collection Vehicle (RCV) movements are to be limited to the streets and access points indicated on the diagram opposite. RCV movements for commercial waste collection will be limited to the same roads used by RCVs serving the residential collections. The service routes have been designed to ensure a maximum carry distance of 10m from any refuse store or presentation area.



Proposed Development masterplan - key points of access and circulation

- - - Emergency vehicles
- Non-service vehicles
- Service vehicles
- Cycles
- . . . Pedestrians

4.6 Development phases

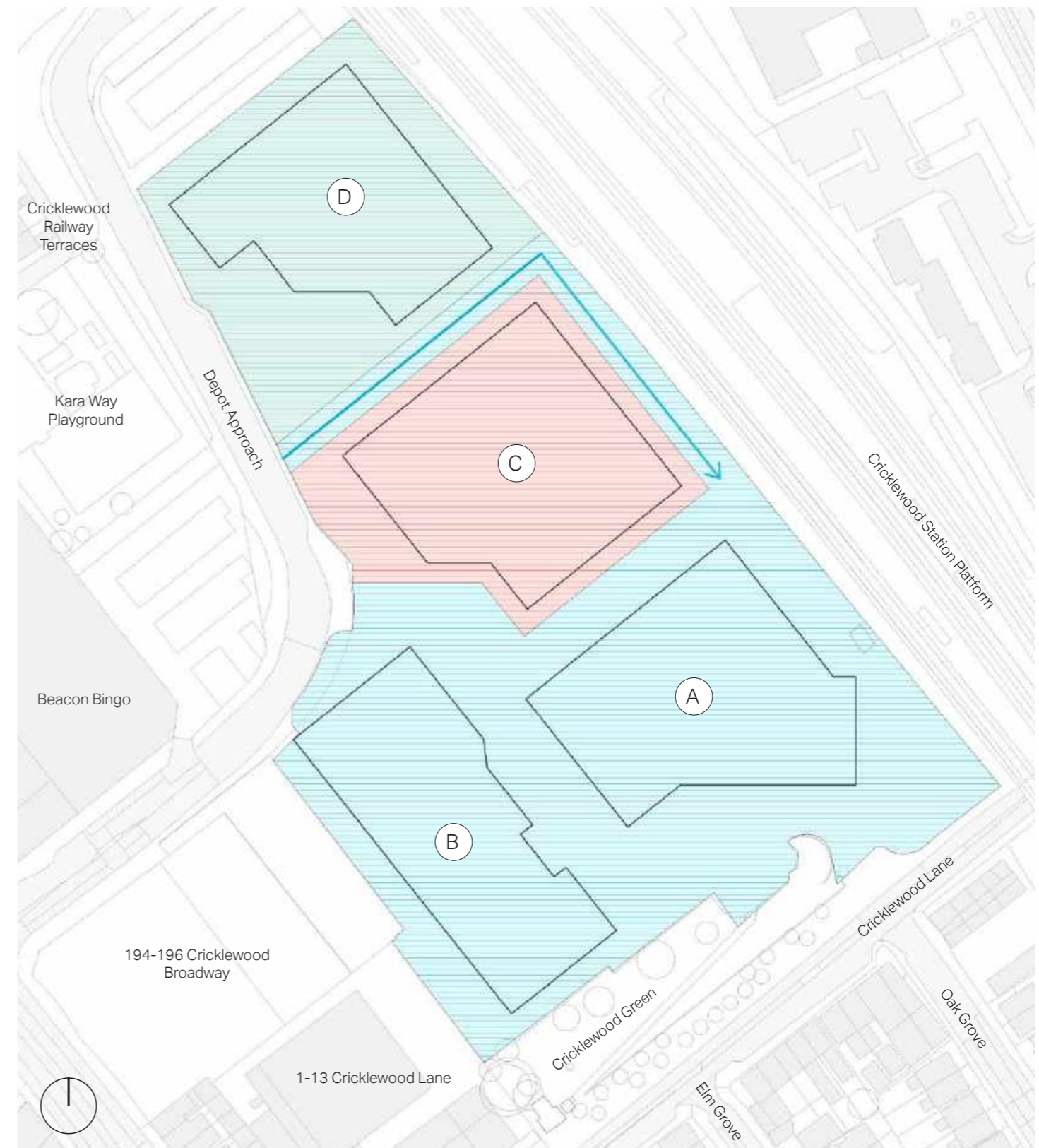
The application includes three Development Phases:

- Phase 1 – Development Parcels A and B
- Phase 2 – Development Parcel C
- Phase 3 – Development Parcel D

The Development Phases are defined on the Phasing Parameter Plan submitted for approval.

An indicative development programme has been prepared in order to enable assessment of the likely environmental effects during the demolition and construction phase of the Proposed Development. The indicative programme is based on a number of assumptions, including the likely phasing of the demolition and construction works technical considerations and professional experience.

It is noted that prior to the start of construction, the phasing of individual plots and areas of public realm to be delivered with each plot will be confirmed.



Proposed Development masterplan development phases

4.7 Wider masterplan

Whilst the masterplan has been designed to be entirely self-sufficient in terms of access it has also been developed to provide flexibility and adaptability to respond to and facilitate future development scenarios on the adjacent Jewson's site to the north.

The future development scenario, which does not form part of this application, but is illustrated to demonstrate how the Proposed Development has sought a stronger cohesive design approach in facilitating its potential inclusion in the aspirations for the wider masterplan.

From the outset, main access (vehicles, pedestrians and cycles) will be gained via Depot Approach. This is referred to as the 'Day 1' scenario.

The 'Day 2' scenario sees the future development of the Jewson site while the 'Day 3' scenario facilitates the delivery of a new landscaped public realm linking the B&Q Cricklewood and Jewson sites, further enhancing the public realm offer for residents of Cricklewood.

The masterplan also provides a safe pedestrian and cycle route through the Site improving the wider pedestrian and cycle network and better connecting Kara Way Playground and Depot Approach to Cricklewood Lane, and the train station.

An aspirational wider masterplan was prepared to explore potential opportunities to improve connectivity and the Site's relationship with the surrounding context and beyond. This demonstrates a holistic design approach which future-proofs the development potential and the emerging spatial framework of Cricklewood town centre.



Wider masterplan strategic development



Day 2 Aspirational wider masterplan developed in collaboration with LBB to ensure the Proposed Development supports possible future development.



Day 3 Aspirational wider masterplan developed in collaboration with LBB to ensure the Proposed Development supports possible future development.



5 Illustrative Masterplan



Adjacent Sketch view of the Cricklewood Lawn within the Illustrative Masterplan



5.1 Use and amount

The Illustrative Masterplan outlined in this statement responds to the Site's opportunities and constraints as well as its emerging context.

The design process has embraced an extensive programme of pre-application consultation, which has influenced the design development of the Illustrative Masterplan.

The Illustrative Masterplan enhances the Site's connection with Cricklewood Green, Cricklewood Lane, the train station and Kara Way playground. It provides a truly mixed-use Proposed Development where new homes and job opportunities can be created.

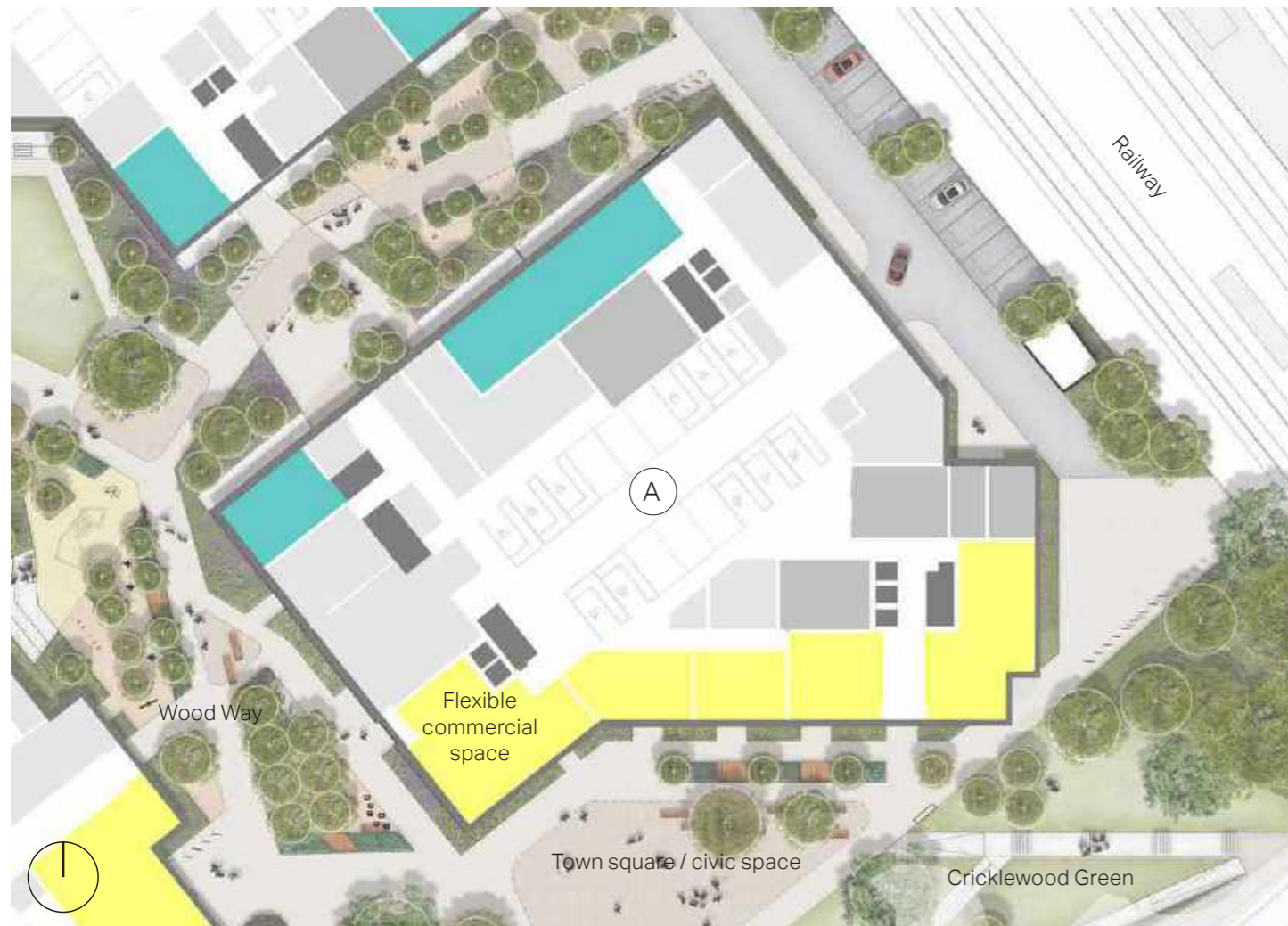
In summary the Proposed Development has the potential to deliver:

- Up to 1,200 sq.m of flexible commercial space;
- Up to 1,100 new homes (of which at least 35% will be affordable);
- New, high quality public realm;
- Up to 110 car parking spaces serving the proposed residential dwellings.

**Areas declared are Gross Internal Areas (GIA) of the RICS Code of Measuring Practice, Sixth Edition.*

Use across the Illustrative Masterplan is predominantly residential with flexible commercial uses provided in key areas at ground floor that offer opportunity to link to and spill out onto public realm. Locating the flexible commercial uses around public realm ensures that the spaces are in use throughout the day, vibrant with activity and supported by passive surveillance from the active shopfront/ workspace frontages.

Building A



Building B



Buildings A and B

The Illustrative Masterplan identifies space within Development Plots A and B suitable for a broader range of uses, specifically:

- A3 - Restaurants and Cafés
- B1 - Business
- C3 - Dwelling Houses
- D1 - Non-residential institutions
- D2 - Assembly and leisure

Ground floor spaces within Buildings A and B around the new town square / civic space and Cricklewood Green can accommodate a range of flexible commercial uses and allow for future flexibility.

Active frontage along Cricklewood Lane extends from the existing offer and wraps around the new public realm drawing people into the town square.

This flexible commercial space could be a mixture of workspace, food and beverage offering and/or community space. Being located closer to

Cricklewood Lane and nearer to the high street, Buildings A and B lend themselves to more active, bustling ground floor uses.

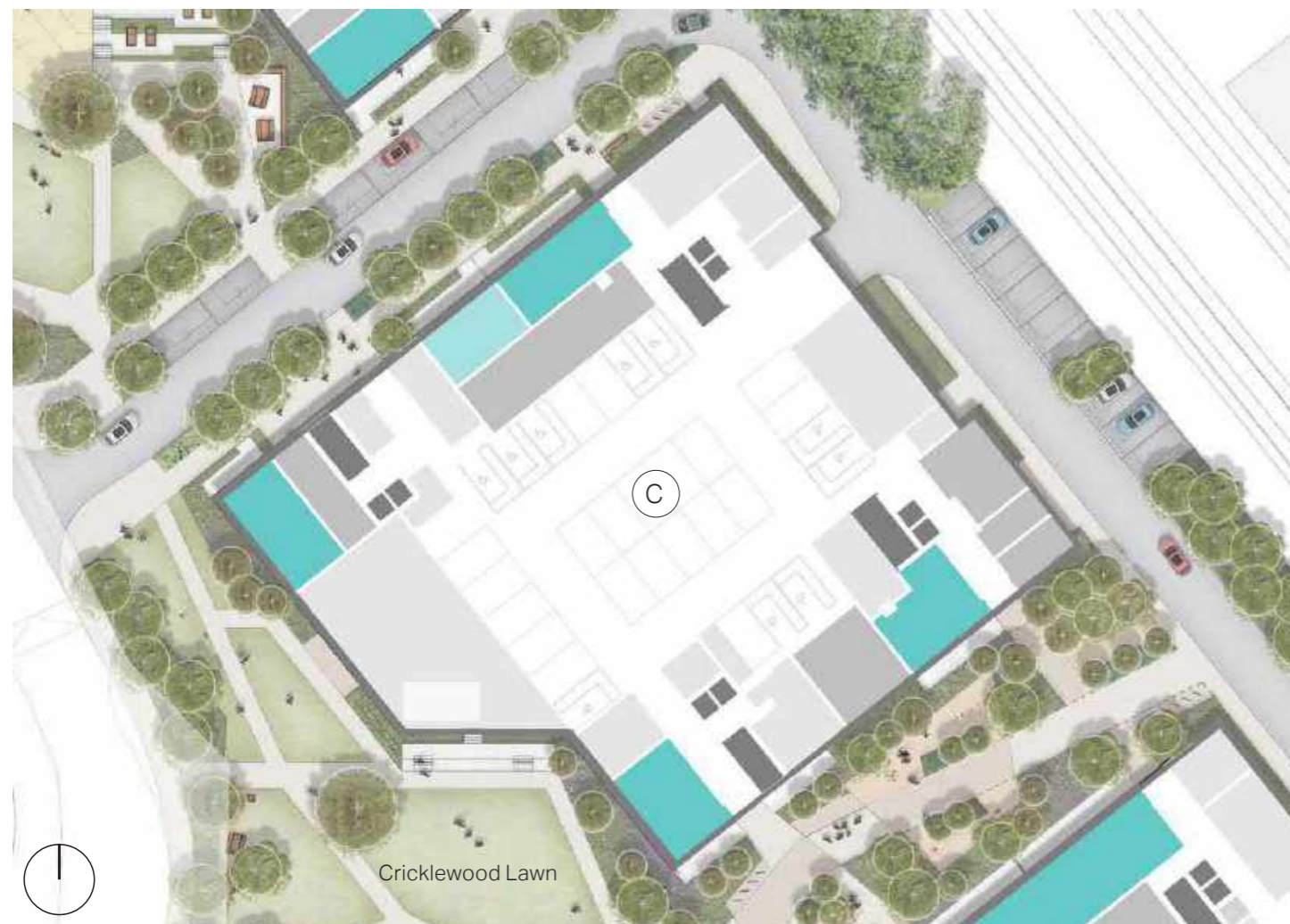
A limited number of dwelling are located at ground floor however where ever these do occur they benefit from direct access from their on front gardens off quieter areas of public realm.

Cycles stores, plant, refuse stores, car parking are all located at ground floor allowing for convenient access and servicing.

Upper floors accommodate majority of the residential dwellings with various offers of shared residents amenity spaces at podium and roof terrace levels.

- Flexible commercial space
- Plant / cycle store / refuse store
- Residential

Building C



Building D



Buildings C and D

The Illustrative Masterplan identifies spaces within Development Plots C and D suitable for the following uses:

- C3 - Dwelling Houses
- D1 - Non-residential institutions

Buildings C and D, set back within the Site, are less exposed to the busy Cricklewood Lane making these Development Parcels more appropriately suited to having residential dwellings at ground floor with private gardens and defensible space taking advantage of the quieter, more protected areas of the Site.

The exception to this being Building D which provides frontage to the proposed play space supporting the existing Kara Way playground. There is opportunity here to provide community facilities that support the new playspace that could be used by residents, the wider Cricklewood community, groups etc. to support and encourage community initiatives.

- Flexible commercial space
- Plant / cycle store / refuse store
- Residential



5.2 Townscape and heritage

Both the masterplan and the Illustrative Masterplan have been informed from an early stage by the input of specialist townscape and heritage consultant Montagu Evans in addition to the height strategy being subject to extensive consultation with LBB Officers.

Below is a brief summary of the key outcomes of Montagu Evans' contribution to the relative strategies to ensure that the townscape benefits arising from the Proposed Development are maximised and that any significant visual effects from introducing tall buildings in this location are mitigated as far as possible.

- The masterplan aspirations for tall buildings on this site have been designed with an understanding of the potential townscape effects both on a local and wider strategic scale;
- The Site does not fall within any of the strategic London Views as identified in the London View Management Framework and is not subject to heritage designations, nor does the Site lie in a protected local viewing corridor. 17 viewpoints have been chosen to test the visual effects of the proposals from local viewpoints and heritage assets;
- The formal heritage, townscape and visual impact assessment carried out as part of the ES submission tests the effects of the proposals on heritage, townscape and visual receptors, using 17 verified views which help to illustrate the effects of the Proposed Development. Montagu Evans assess the acceptability of the proposals in line with London Plan policy 7.7 and local plan policy DM05.;

- The height strategy of the Proposed Development ensures greater height is positioned along the railway, significantly stepping down in scale towards the north west of the Site and down towards Woodway running through as the spine of the Proposed Development. The tallest building adds legibility to the Proposed Development being located nearest to Cricklewood Station and the proposed public realm;
- Where the Proposed Development is seen from the Cricklewood Railway Terraces Conservation Area and the Grade II listed Crown Public House, these comprise glimpsed views of the taller elements of the buildings, which appear and disappear in these transient views;
- Montagu Evans conclude that there will be significant townscape benefits arising from the proposals. There will not be any unacceptable impacts on the setting or value of heritage receptors. Where some impacts on heritage receptors have been identified, these are not significant and arise only from an element of intervisibility between the receptors and the Proposed Development. Montagu Evans conclude that the value of the receptors is not affected and that in NPPF terms, no harm is caused to designated heritage receptors (assets). There will be significant changes to some local views (undesigned) as a result of the regeneration of the Site. These changes and the visibility of the tallest elements on the Site signal the regeneration of the Site and the positive changes brought to the neighbourhood in returning the Site back into active use.



5.3 Access and circulation

Both the masterplan and the Illustrative Masterplan have been informed from an early stage by the input of specialist transport consultant Entran.

The below text is a brief summary of the key outcomes of Entran's contribution to the relative strategies.

The outline planning application is supported by a Transport Assessment which demonstrates that the Proposed development will result in a significant reduction in vehicle movements when compared to the existing retail use of the Site. This will in itself result in an overall improvement in local highway conditions. However, the reduction in vehicle trips also provides the opportunity to serve the Proposed Development from a single vehicle access and remove the existing limited-movement access onto Cricklewood Lane. This will benefit highway safety but also provide an improved and less car-dominated environment for pedestrians and cyclists.

The movement strategy provides high quality public realm through the heart of the scheme by locating service routes and car park accesses around the perimeter. The central pedestrian and cycle routes are therefore designed using the Healthy Streets principles.

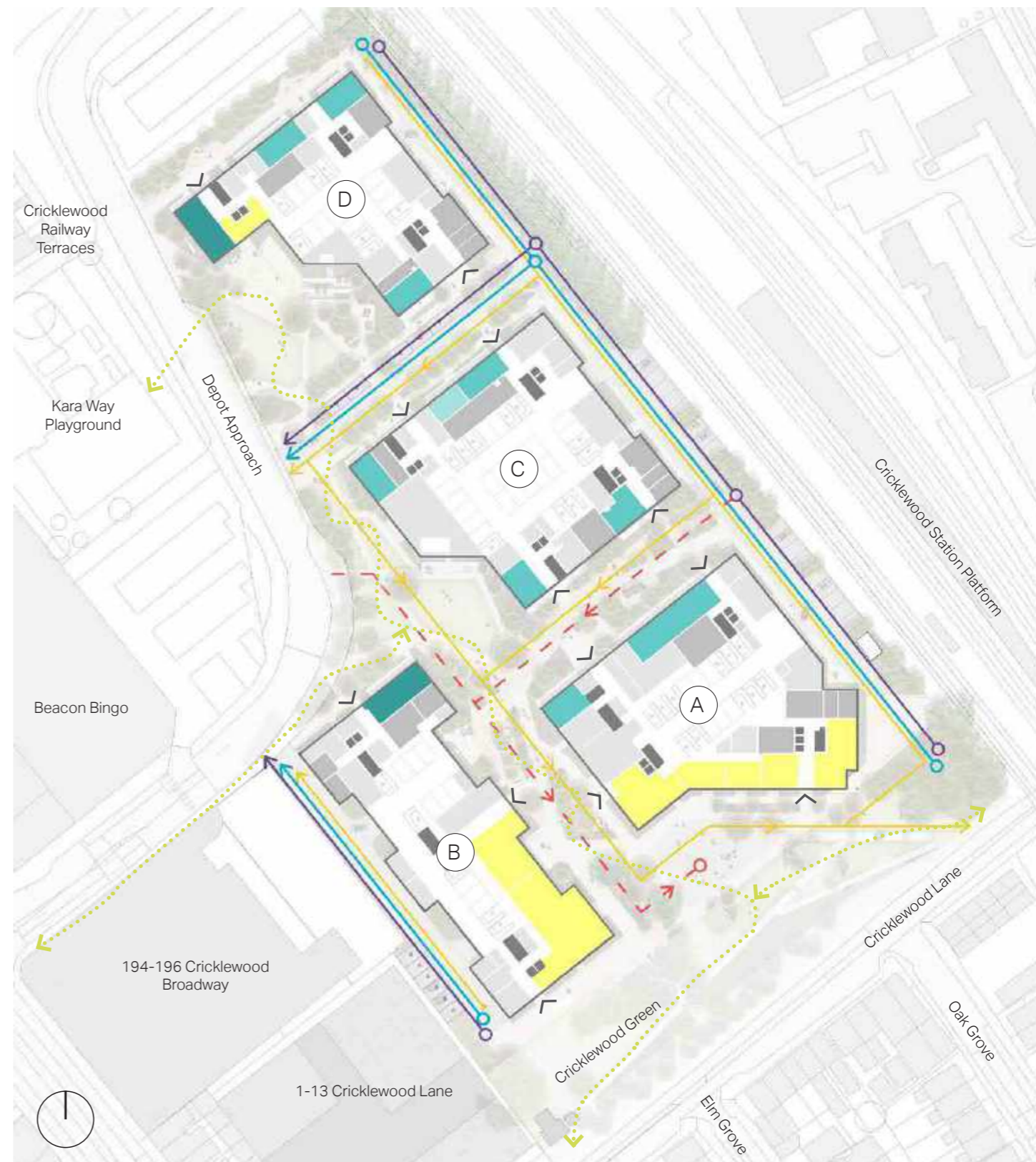
The new direct route through the Site will reduce walking and cycling distances to key locations such as Cricklewood Station and Cricklewood Broadway. This direct route will increase the PTAL rating of the rear portion of the Site as well as improve the accessibility of land to the north-west beyond Kara Way playground.

The Proposed Development is supported by a three-part Transport Implementation Strategy (TIS) comprising a Framework Travel Plan, Delivery and Servicing Plan and Construction Logistics Plan. This approach will ensure that the movement of people and goods is managed from the outset and forms an integral part of the design process. The object of the TIS is to promote sustainable travel behaviour through good design, quality infrastructure, provision of information, and incentives to encourage sustainable travel choices.

The Proposed Development will have minimal residential car parking (10%) and only operational parking for the commercial and community uses. At least 20% of the car parking will have electric vehicle charging points and the remainder will have passive provision for future expansion subject to demand. The Proposed Development will deliver a new Car Club space for the benefit of the new residents as well as the local community.

The Proposed Development will capitalise of the Site's very good accessibility. Every new home will be provided with secure cycle parking in accordance with the London Cycle Design Standards further and short-stay cycle parking will be incorporated into the landscape design, close to building entrances.

- Emergency vehicles
- Non-service vehicles
- Service vehicles
- Cycles
- Pedestrians



Proposed Development masterplan - key points of access and circulation

5.4 Waste strategy

Residential waste strategy

To manage the waste and recycle materials arising, separate bin stores have been provided at ground floor serving each residential core. These bin stores are located within 30m from the residential dwellings they are serving in compliance with BS 5906:2005 and LBB guidance.

To efficiently manage space within the bin stores and to provide a robust solution for the waste and recycle materials arising, lever arm in-bin compaction units will be utilised where possible.

Any bin store that cannot house the in-bin compaction units due to space constraints, will be monitored by an internal management team. Once bins are full within these stores, the internal management team will transfer these to the closest compaction units within the building. After compaction, the bins will be returned to the bin store.

The process of waste and recycle disposal will follow a conventional approach. Residents will access bin stores (via lifts) to dispose of waste and recycle materials in the appropriate bins (MDR, Food and Residual). For safety, in-bin compactors will be caged. In addition, these compactors will be operated by trained members of the internal management team.

At collection, the Refuse Collection Vehicle (RCV) will park within 10m of the bin stores and the collection crew will transport the bins directly from the bin store to the RCV for emptying. Once emptied, the collection crew will return bins back to the bin store.

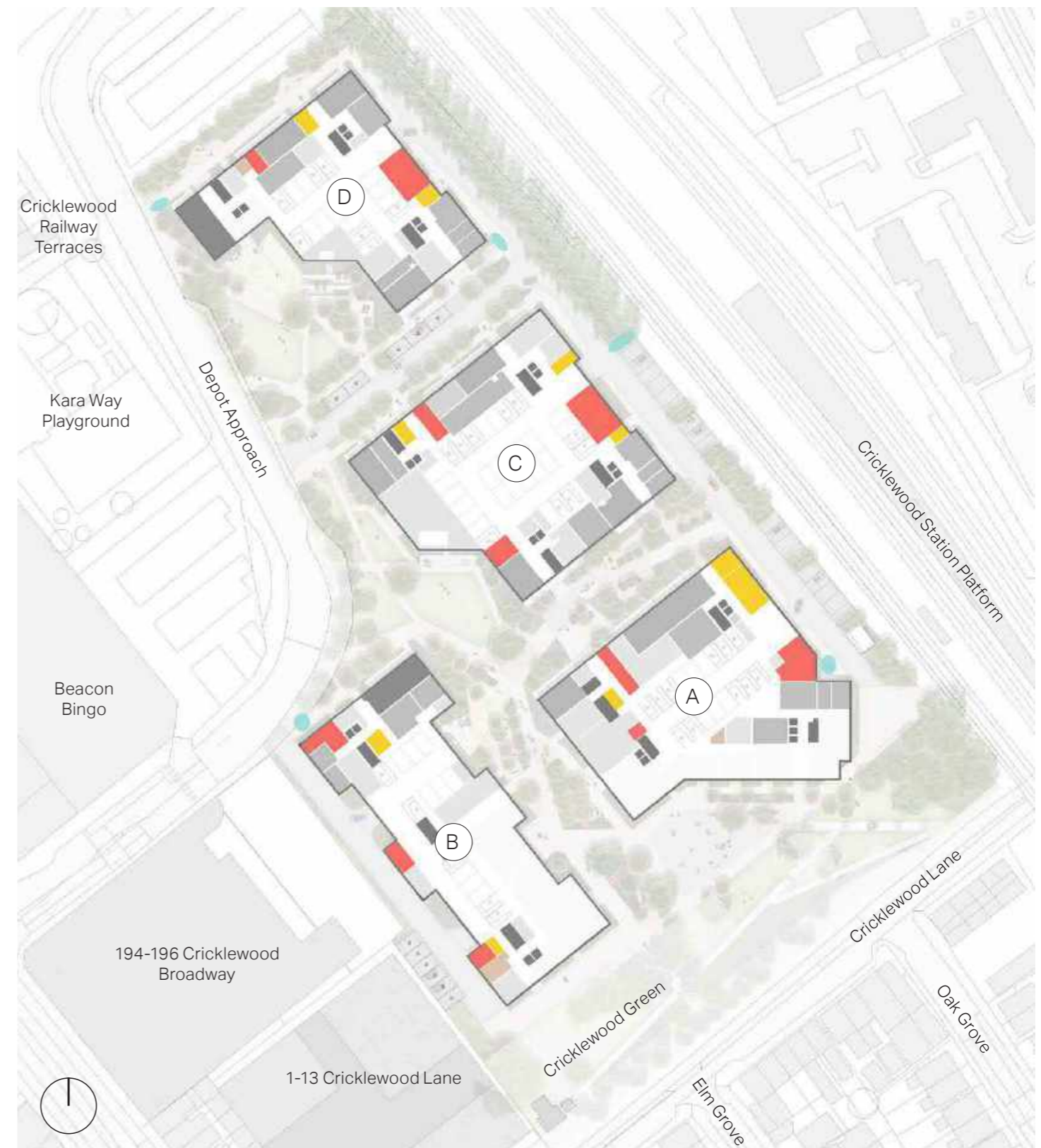
Bin stores which exceed 10m distance from the parked RCV will require the internal management team to wheel the bins to a designated presentation area. Bin stores that exceed 30m from the presentation area will require the internal management team to use tugs for the transportation of bins to the presentation area. From the designated presentation areas, the collection crew will transfer bins to the RCV (parked within 10m of the presentation point). Once bins are emptied, the collection crew will return bins to the presentation area. The internal management team are responsible for returning bins back to the bin stores.

Commercial waste strategy

Commercial bin stores will be provided in the relevant buildings to accommodate commercial waste and recycle arising. Commercial bin stores will provide sufficient space to hold bins for all waste streams (MDR, Food and Residual waste) based on weekly collection frequency. Daily (or as agreed), the internal management team, or staff, will collect waste and recycle material from all commercial units and dispose of this waste and recycle material into the bin stores.

Prior to the collection time (or as agreed), the internal management team will transfer waste and recycle bins for the waste stream (MDR, Food or Residual waste) scheduled to be collected to a designated presentation area (located within 10m of the RCV collection point). Stores exceeding 30m from the presentation area will require the use of tugs for transportation of bins. From the designated presentation areas, the collection crew will transfer bins to the RCV, empty and return bins to the presentation area. The internal management team are responsible for returning bins back to the correct bin stores.

- Commercial refuse store
- Residential refuse store
- Bulky waste store
- Collection / presentation area



Illustrative Masterplan waste strategy



5.5 Fire safety

The Illustrative Masterplan has been informed by the fire strategy, developed by specialist fire consultant, Elementa Consulting.

The below text is a brief summary of the key considerations and outcomes that have been taken into account in the Illustrative Masterplan:

- The residential accommodation shall be designed in accordance with BS9991 whilst the commercial units shall be designed in accordance with Approved Document B which addresses fire safety precautions;
- The residential accommodation shall undertake a stay-put policy where only the affected apartment shall be evacuated;
- Where the travel distances in the common corridors are limited to 15m, the corridors can be ventilated either naturally or mechanically, with the smoke shafts located anywhere in the corridor. In the case of Building B, since the travel distances are extended, the common corridors shall be ventilated via a mechanical smoke ventilation system, with the smoke shaft provided at the dead end of the corridor;
- The cores that serve a floor over 18m in height shall be provided with a firefighting shaft incorporating a firefighting stair, firefighting lift and dry rising main (in the staircase). To satisfy the hose distances, the cores that do not serve a floor over 18m in height will be provided with a dry riser (in the staircase);
- Buildings under 18m in height shall be provided with 60mins fire resistance to the elements of structure. Buildings with a floor over 18m but less than 30m shall be provided with 90mins fire resistance to the elements of structure. Buildings with a floor over 30m shall be provided with 120mins fire resistance to the elements of structure;
- The apartments in all cases shall be enclosed in 60mins fire resistance;
- The escape staircases shall be enclosed in the same fire resistance as the elements of structure unless they are designed as a firefighting stair, in which case, they will require to be enclosed in 120mins fire resistance;
- Car parks, amenity spaces / ancillary spaces and flexible commercial units shall undertake a simultaneous evacuation strategy of the affected accommodation only;
- Car parks shall be accessed via a protected ventilated lobby from the residential accommodation. Additionally, car parks will be required to be provided with ventilation to both dissipate the build-up of car fumes and to provide smoke clearance. This could be achieved naturally (via the provision of openings) or mechanically;
- Flexible commercial units shall be designed to be completely independent from the residential accommodation. They shall be compartmented from the residential accommodation with the same fire resistance as the elements of structure; and
- Where a building has a floor over 30m in height, it shall be provided with a sprinkler system. The sprinklers will need to be provided in the entire building (i.e. including amenity spaces, flexible commercial units and car park areas).



5.6 Daylight and sunlight

Both the masterplan and the Illustrative Masterplan have been informed from an early stage by the input of specialist daylight and sunlight consultant GIA.

Below is a summary of the key outcomes of GIA contribution to the relative strategies:

The massing has been arranged so as to minimise the impacts on neighbouring properties as well as to allow for good levels of amenity within the proposed accommodation and open spaces.

The tallest buildings have been located along the rail tracks, stepping down towards the neighbours and several visual corridors are provided between the buildings to increase daylight and sunlight permeability.

Owing to the outline nature of the application, façade assessments have been undertaken on the Illustrative Masterplan to inform the massing design and to establish how the daylight and sunlight amenity within the Site can be further optimised at future RMA stages, when internal layouts and elevations will be fully resolved.

The outer frontages enjoy a mostly unobstructed outlook and therefore the majority of façades achieve very good daylight potential. In these areas, a conventional design of the elevations and layouts is likely to lead to good levels of daylight within the rooms. As is typical of any dense scheme, where two parallel façades are in close proximity of one another or within inner corners of courtyards, the access to daylight is inevitably more restricted. A number of strategies have therefore been explored that will allow to maximise the daylight performance, and these should be further developed at future RMA stages, when a sufficient level of detail will be available for a full testing to be undertaken.

Levels of sunlight are also good across the Proposed Development, with the majority of southerly elevations meeting or exceeding recommended sunlight minima across the year and during winter months.

The Proposed Development Illustrative Masterplan also provides a range of public and communal outdoor spaces at ground, podium and on roof terrace level, whose articulation has been informed by a number of overshadowing assessments. As a result, the vast majority of the open spaces provided will see good or excellent levels of sunlight, providing good quality spaces for the enjoyment of future occupant and of the wider community.



Adjacent Sketch view of Wood Way, the central green route through the Illustrative Masterplan



5.7 Energy and sustainability

Both the masterplan and the Illustrative Masterplan have been informed from an early stage by the input of the Energy and Sustainability Consultant, Meinhardt.

The below outlines the key outcomes of Meinhardt's contribution to the relative strategies.

The Proposed Development's design will reflect that of the London Plan Energy Hierarchy, of 'be lean, be clean and be green', whereby passive measures are included first by appropriate orientation of buildings, limiting the amount of direct south and west facing façades. Low U-values and a balanced glazing to solid ratio within the façade build-up will limit the overheating potential of the dwellings whilst restricting heat loss. The apartments will include for opening windows and be designed to encourage cross ventilation with mechanical ventilation incorporating heat recovery (MVHR) to provide background means of ventilation.

The Illustrative Masterplan is designed to be future proofed to connect to a district heating network, should it ever become feasible to deliver one to the Site. However the constraints of the Site located next to the rail line limit the current potential as both the nearest district heating networks (Brent Cross and Hampstead) are the opposite side of the mainline train route. In light of this, the Illustrative Masterplan will adopt a community heating network, initially designed for low carbon heat via heat pump technology. Each building will have its own heat pump set producing low temperature communal heat which then distributes around the Site to be utilised within the dwellings and commercial floor space. This will be initially adopted as a low temperature heat network which reflects the latest revisions of Part L and SAP 10. This also limits the centralised overheating risk within the communal corridor areas with flow and return temperatures of 25/15 oC. The communal heat pump locations per block have been appropriately identified for the most appropriate free air circulation and too limit background noise.

Finally solar photovoltaics will be provided in any unused roof space to provide a renewable energy contribution. This strategy will see a Carbon Dioxide reduction of circa 40% from the latest Building Regulations. It is proposed that carbon offsetting will then be calculated for the residential element for achieve zero carbon status.

Internal potable water use shall be restricted to 105 l/p/d by the specification of low flow fittings per apartment and rainwater recycling shall be utilised for amenity areas watering.

The non-domestic areas shall be designed to achieve a BREEAM 'Excellent' rating which incorporates sustainable use of materials and associated landscaping shall benefit the local ecological value to the Site.

5.8 Wind micro-climate

Both the masterplan and the Illustrative Masterplan have been informed from an early stage by the input of specialist daylight and sunlight consultant RWDI.

The below outlines the key outcomes of RWDI contribution to the relative strategies.

Wind microclimate studies for the Proposed Development have been undertaken using wind tunnel testing during the design development process. The results of these studies have allowed recommendations to be made regarding the design of the Proposed Development, as set out within the Design Guidelines.

As part of the design evolution and in response to wind tunnel testing, proposed measures, such as architectural and landscape details, to be implemented as part of the Proposed Development at the detailed design RMA stage include landscaping, such as the addition of shrubs at least 1.5m high and trees ranging from 3m to 7m high or screens/art work covering an area of 2m² in place of trees. Entrances that are located in areas of unsuitable wind conditions will likely need to be recessed by 1.5m or the addition of side-screens (2m high by 1.5m wide) to ensure users do not encounter unsuitable wind conditions at the entrances. The majority of roof terraces would require balustrade at least 1.5m high with soft or hard landscaping elements distributed on the roof top.

5.9 Materiality

The existing setting of the Site is characterised by Victorian, Edwardian and modern developments (which range in height from three to five storeys), retail units and the well-used thoroughfare.

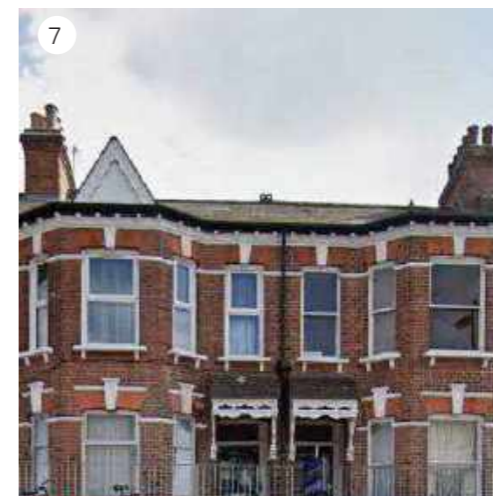
To the north and east built form comprises buildings of between three and four storeys with glazed shopfronts at ground floor and residential flats above. Built from red, gault and yellow stock brick, these buildings are terraced and have consistent building and roof lines.

Architectural features include classical detailing in stone and plaster, sash and casement windows, parapet walls, dormers and mansard and gabled roofs.

By virtue of their complementary architecture, materials and uniform elevations, the early-C20 buildings positively contribute to the heritage value of the area, although cluttered commercial signage and non-original fenestrations lessen this somewhat.

Future RMAs should carefully consider the existing material and architectural features of the surrounding context including:

1. Shared residents' green space;
2. Distinct horizontal banding;
3. Consideration of historic buildings;
4. Window and roof ornamentation;
5. Differentiated base;
6. Varying material palette with consistent facade proportions;
7. Expressed window frames and sills; and
8. Grouping of windows



Existing materiality and architectural features of the surrounding context

The Proposed Development should be of exemplary design quality. Consideration should be given to the overall approach to materiality and colour palette for the whole site, to ensure each building coming forward is an appropriate fit within the emerging neighbourhood.

A limited palette of materials would help to ensure a coherent architectural language is established.

In keeping with the existing architectural context, the primary building material should be brickwork, to provide a consistent aesthetic with a robust finish, uniting the architectural language of the different buildings with the surrounding context.

All materials should be durable and easy to maintain to ensure a high-quality finish over the life span of the development.

While the primary facade material is brick, subtle variation in tone should differentiate between buildings providing a sense of unique identity and adding variation to the overall Proposed Development. In particular, the buildings enclosing the new town square should reflect and reinterpret the distinct architectural character of Cricklewood Broadway i.e. the red brick tones with contrasting secondary white detailing.

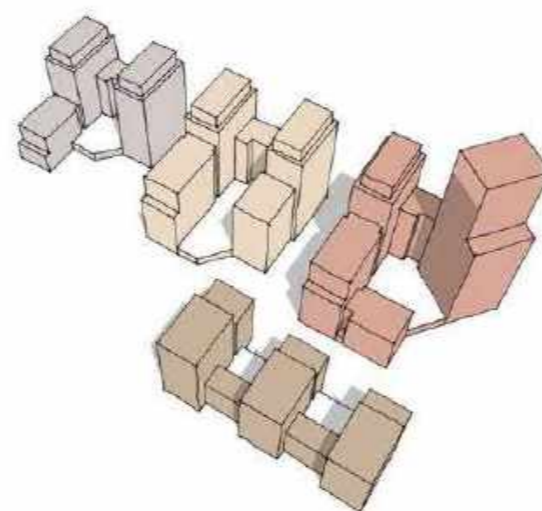
Across all Development Plots, the use of colour and texture should allow for flexibility and design expression to achieve a coordinated variety of architectural treatments.

A contemporary aesthetic influenced by the existing materiality and architectural features of the surrounding context should be explored including:

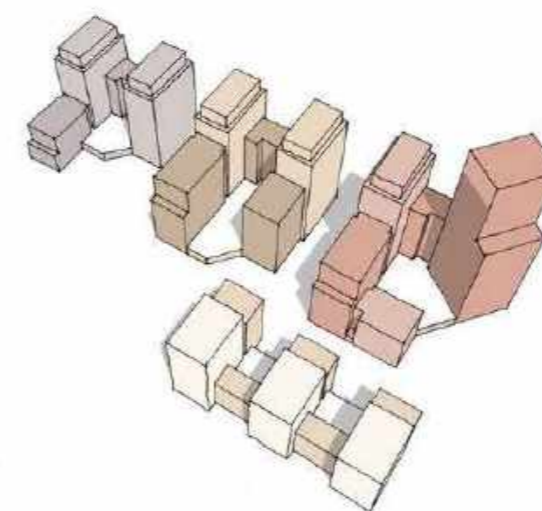
1. New materials complementary with existing;
2. Distinct horizontal banding;
3. Relationship with historic buildings;
4. Grouping of windows with detail panels;
5. Contrasting window framing;
6. Expressed sills;
7. Variation in material tones; and
8. Contemporary ornamentation to key areas.



Contemporary and appropriate interpretations and expressions of the existing materiality and architectural features of the surrounding context



Complementary variation in brick tones for individual Development Parcels



Subtle variation in brick tone within individual Development Parcels



A consistent approach to materiality with subtle shifts in architectural expression will unify the buildings across

Sketch view of the Illustrative Masterplan and new town square / civic space and possible enhancements to Cricklewood Green, used for curated community events and gatherings







KARA WAY
PLAY AREA



Adjacent Sketch of the Illustrative Masterplan and playspace in front of Building D

6.1 Introduction

Exterior Architecture Ltd (ExA) has been appointed by Montreaux Cricklewood Developments Ltd (the Applicant) to formulate the landscape proposals for the Proposed Development of B&Q Cricklewood, London.

This section of the Design and Access Statement sets out the landscape strategy for the various spaces that constitute the Proposed Development, its composition and design approach.

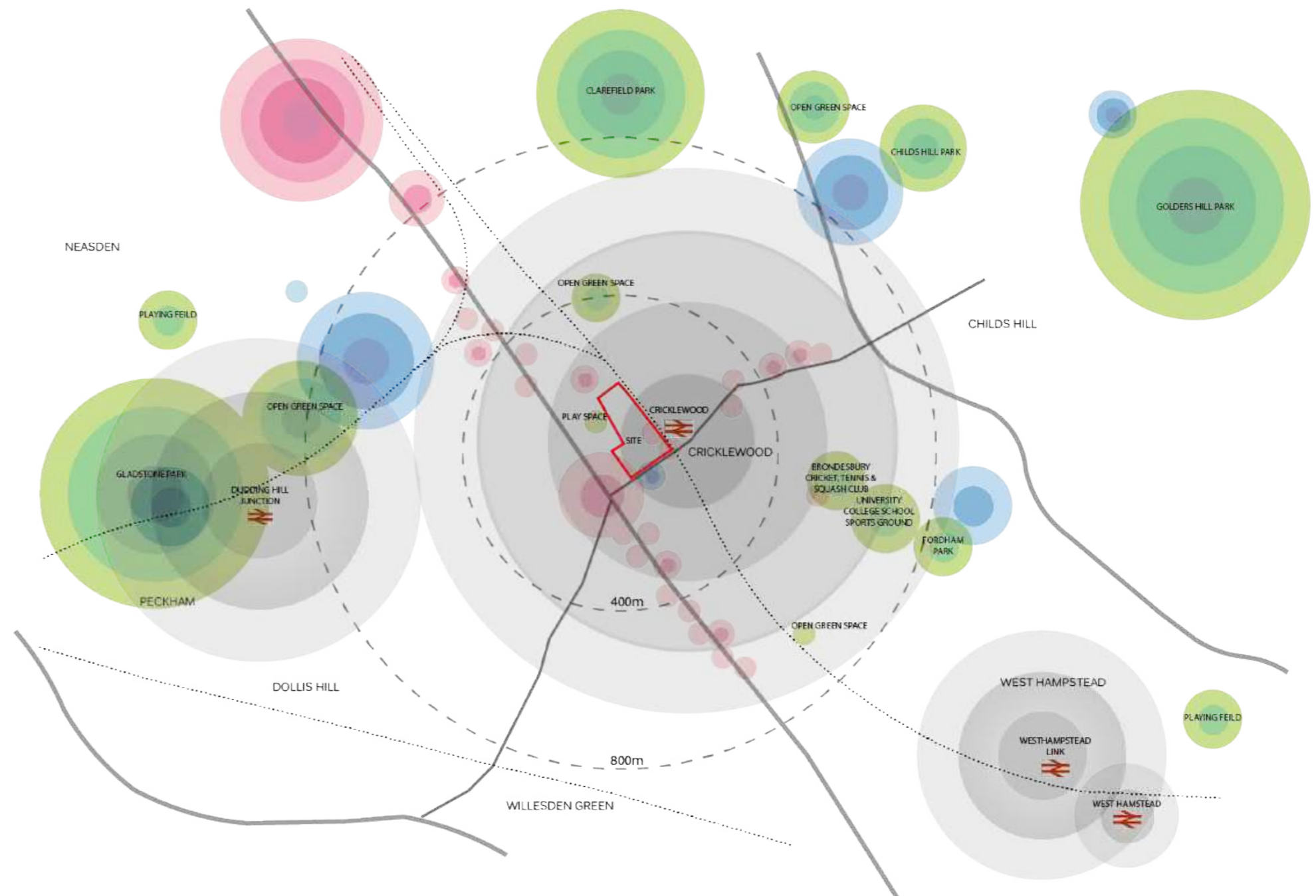
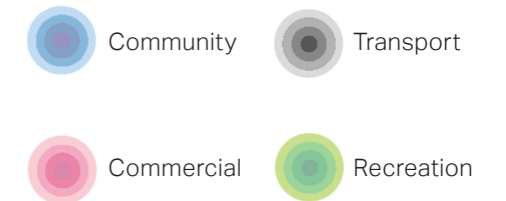
ExA have developed the landscape proposal in collaboration with the wider design team and in consultation with key stakeholders including London Borough of Barnet (LBB) and local residents, and the contents of this section reflects this approach to team working.

This section should be read in conjunction with the associated application drawings and all other information submitted in support of the planning application.

6.2 Site context

The Site is strategically located on Cricklewood Lane by the busy Cricklewood Train Station which serves connections into central London and towards Luton Airport. Cricklewood Broadway near the Site is active with a high footfall of pedestrians and traffic directly in front of the Site.

Cricklewood Green faces Cricklewood Lane and is a well used local green space. However, the wider area does have a lack of civic space or a community platform that can be used for events and gatherings and as a focus point of local community activity.



6.3 Site history

Cricklewood lies in a valley between five hills with five streams (all culverted) and, like most areas surrounding London, has a long history of settlement.

Notable heritage includes;

- **Crickle Wood :**
"a wood with an uneven edge"
 - name given by the Saxons

By 1294 :

- First settlement

17 and 18th century :

- Woodland cleared in favour of new farm land and economic growth

1815 onwards :

- Construction of 36 houses and villas

1870 :

- Construction of railway and station

1893-1900 :

- Construction of "Cricklewood Park" with streets named after trees
- First factory in the area (imperial dry plates works expanded until 1940)

1900-1914 :

- Cricklewood Broadway built - shopping parade in "metropolitan electric" style

Circa 1900 :

- Identity and community growth with churches and associations (improvement, horticulture, newspaper)

1915-1930 :

- North expansion for new industries (aircraft, telephone, film studio, cars...)



1761

"Krickle Wood"



1895

Child's hill and Cricklewood station (1870) and "Child's hill sidings"



1915

Cricklewood station



1936



Hamsterley Trees



Cricklewood Railway Station



Cricklewood Broadway



*The Beast of Barnet
Urban Legend*

6.4 Opportunities and constraints

The Site comprises an existing B&Q store and carpark with good connections to existing transport (Cricklewood Station) networks to Central London. The Site is a prime location for development to utilise these connections and to provide new community open space and community assets for existing and new residents.

KEY

DESTINATIONS

-  Existing key transport destination
-  High Street commerce
-  Existing community destination
-  Existing local garden/play destinations
-  Proposed Community destination
-  Proposed activity node
-  Proposed Play destination
-  Proposed Play nodes



MOVEMENT

-  Bus + Car road
-  Bus stop
-  Car only road
-  Key pedestrian link and possible inclusion in the Dollis Valley Greenwalks
-  Potential extension of pedestrian link
-  Key view

LANDSCAPE/ECOLOGY

-  Existing trees retained
-  Railway habitat
-  Existing public green space
-  Proposed public green space

LEVELS

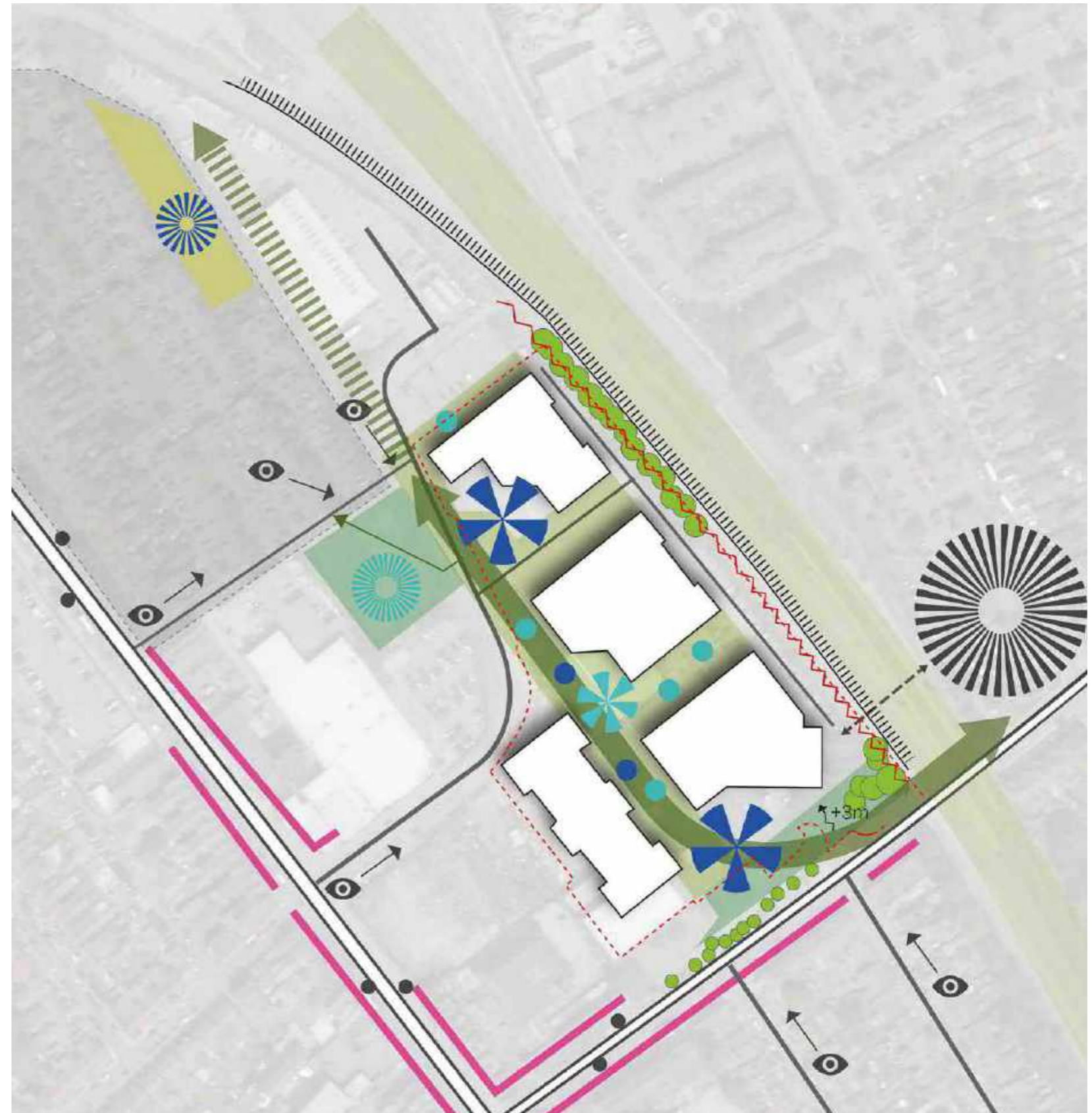
-  Existing embankment
-  Railway wall/boundary tbc

DESIGNATIONS

-  Conservation Area

OTHERS

-  Noise

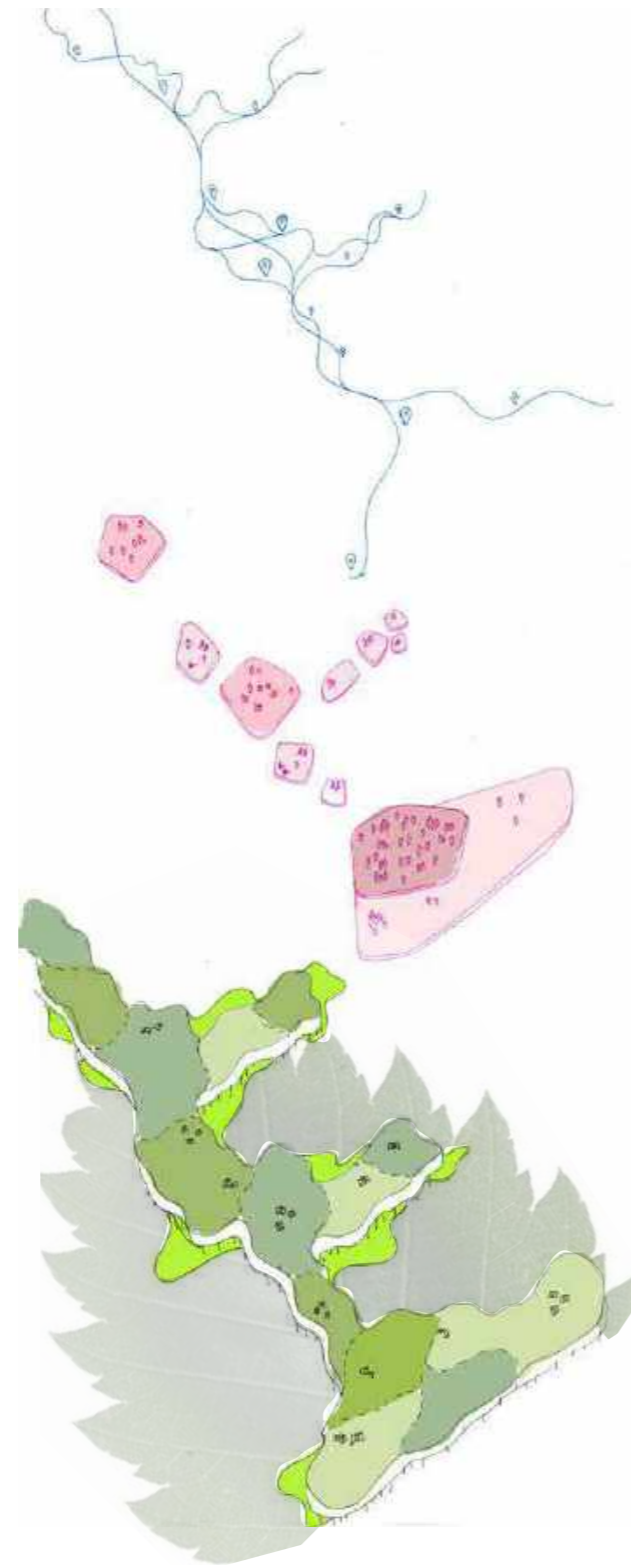


6.5 Vision

As well as providing new community, commercial and residential spaces, the Proposed Development should create new landscape and street typologies that provide places to sit, rest and socialise. Focussing on creating recognisable and distinct spaces, the landscape provides a community asset that can be enjoyed by residents, neighbours and visitors.

The landscape for the Proposed Development is envisioned as an activated and immersive storytelling mechanism with three themed layers. These interwoven themes create a cohesive public realm design that evokes historical connections and promotes new types of activation.

Overarching the design of public realm at Cricklewood is a three layered vision which aligns site history and stories (The Tales) with social (The Glades) and ecological (The Woods) considerations. These themes are central to the design of the public realm and should continue to permeate the design.



The Tales

- The design of the public realm is artistic and imaginative
- The design of the public realm promotes urban exploration through key way-finding mechanisms and is a succession of spaces with views and surprises
- The design of the public realm references local stories and history through art, play elements and trails

The Glades

- The design of the public realm creates community platforms of different scales and uses for residents, neighbours and locals
- The design of the public realm promotes biophilia and physical and social well-being
- The design of the public realm utilises the variety of atmospheres and light levels to inform site characters and planting strategies

The Woods

- The design of the public realm seeks to reference site history and surrounding neighbourhood character
- The design of the public realm utilises mechanisms to denote the Proposed Development as 'a wood with an uneven edge'
- The design of the public realm enhances wildlife habitat and is ecologically-minded and rich in biodiversity



6.6 Landscape objectives

The following objectives, along with the vision layers of The Tales, The Glades and The Woods, inform the design of the public realm:

- **A civic heart with a community focus;**

To ensure a high-quality finish over the life span of the Proposed Development.

- **An aspirational place to settle**

To ensure safe and comfortable residences and outdoor/public realm areas that cater to a variety of users.

- **Links and connections through the Site**

To integrate the Site with its surroundings and provide paths that connect the existing street network with the Site layout.

- **Generous publicly accessible green space**

To contribute and enhance the existing green infrastructure network.

- **A succession of spaces and experiences**

To showcase a variety of new spatial typologies within Cricklewood.

- **One visible and generous civic space**

To provide a public area for a variety of community gatherings and curated events.

- **A green pedestrian route**

To encourage active travel and recreation.

- **Varied views and elements of surprise**

To provide a diversity of site experiences.

- **Visual connection with podium gardens**

To showcase how the public realm and architecture can be symbiotic and provide continued activation at varying levels.

6.7 Illustrative Masterplan

The Illustrative Masterplan seeks to establish a vision and framework for development across the Site. The plans on the opposite page are illustrative only and have been developed alongside EPR Architects using a landscape-led approach.



6.8 Character areas

As a mechanism of distinguishing between landscape typologies, the public realm is divided into a series of landscape character areas. These present an opportunity to create spatial variety within the public platform. A structured palette of materials should create visual unity across these Character Areas while connecting the detail of the architecture with the material and activation of the ground plane.

Information and graphics regarding Cricklewood Green in this part of the document set out an indicative proposal for how the Green could come forward in the future to complement the wider Masterplan.

Arboretum Place

Arboretum Place provides a high quality hardscaped civic space which provides for multiple community interactions and curated events to take place.

Cricklewood Green

There is opportunity to enhance Cricklewood Green in the future to complement the Proposed Development.

Wood Way

Wood Way is a high quality, well-vegetated, active and ecologically diverse succession of pedestrian-centric spaces between Development Parcels

Cricklewood Lawn

Cricklewood Lawn is a high quality green open space for recreation and gathering

Rail Side

The Rail Side provides service and vehicular access to buildings adjacent the rail corridor



Arboretum Place

Arboretum Place provides a high quality hardscaped civic space which provides for multiple community interactions

Arboretum Place is the civic heart of the Proposed Development; it is a new public square for Cricklewood where community activities and gatherings of varying sizes can occur. Flanked by active frontages, and overlooking Cricklewood Green, Arboretum Place is the most 'urban' of the proposed character areas and could complement Cricklewood Green and create a flexible platform for greater possibilities of community activities across both spaces thus increasing the public space available to the residents of Cricklewood.

The open nature of the space is softened with lush planting in raingardens, large canopied clear stem trees and seating elements that edge the perimeter of the space. There is the opportunity for spill-out from the adjacent flexible commercial spaces so that retailers and workers can take ownership of the edges of Arboretum Place and appropriate it as their own.

As the name suggests, the tree diversity serves as a defining feature of this civic space, in reference to woodland inspired public realm and the surrounding streets named after trees. The trees also provide a visual and ecological link between Cricklewood Lane, Cricklewood Green and the Wood Way.

- 1 Flexible open space
- 2 Grove of trees
- 3 Seating area under the trees
- 4 Sculptural play element
- 5 Spill-out space



Key Principles

- Hardstanding flexible central area for gatherings and events such as food markets, outdoor cinemas and exhibitions overlooking Cricklewood Green;
- Species-rich shrub, grass and herbaceous planting in raingardens or in-ground planter beds which define the edges of the flexible central area and provide a green periphery;
- Semi-mature clear stem deciduous and evergreen specimen trees in both hardscaping and softscape which complement the proposed spatial uses and frame the area of hardstand;
- Minimum 3m commercial spill-out and alfresco dining spaces to co-ordinate with flexible commercial spaces;
- Groves of multi-stem trees and planting which connect to the character of The Wood Way;



- Under-canopy seating to accommodate a variety of small group sizes;
- Artistic and sculptural play elements within proposed seating and planted areas;
- High quality surface treatment.

Cricklewood Green

There is opportunity to enhance Cricklewood Green in the future to complement the Proposed Development.

Potential enhancements to Cricklewood Green, a recognised Asset of Community Value, could seek to enhance this existing local community asset to increase its usability, accessibility, safety and biodiversity while retaining its character and current uses.

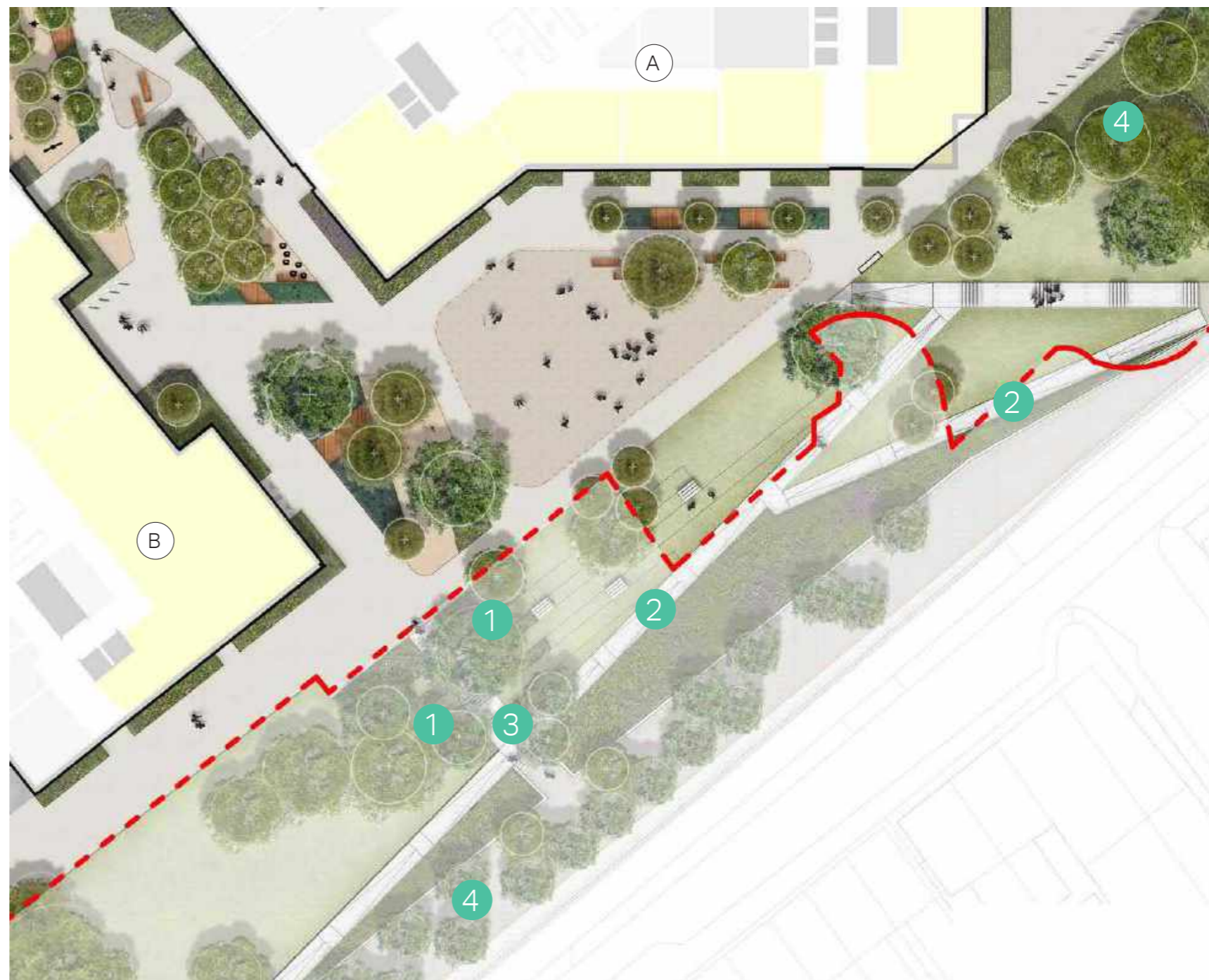
The grass slopes could be turned into a planted terraced lawn with accessible routes from Cricklewood Lane to Arboretum Place and the Wood Way. Taking advantage of the existing change in level, the introduction of stepped access would generate the opportunity within the landscape to create south-facing seating terraces in the centre of the Green which can be utilised on market days by visitors.

The introduction of terraces would also increase the day-to-day usability of the Green to create spaces for people to sit, rest, observe and socialise, while increasing safety with the passive surveillance from the proposed buildings. Existing mature trees and community-led planting near the railway could be retained.

- 1 Levelled terraces
- 2 Step-free routes (>1:21)
- 3 Step access
- 4 Existing trees retained

Key Principles

- Visual character of the Cricklewood Green lawn area to be maintained.
- Additional stepped access up to Arboretum Place from Cricklewood Lane provided within the existing green.
- Existing trees are to be retained, where appropriate.
- Semi-mature clear stem deciduous and evergreen specimen trees in softscape frame the new access routes and provide necessary canopy cover.
- Provision for species-rich shrub, grass and herbaceous planting on sloped areas <1:3.
- Seating elements to be primarily facing Cricklewood Lane.
- Artistic and sculptural play elements on lawn areas.





Concept sketch of Cricklewood Green and Arboretum Place

The Wood Way

The Wood Way is a high quality, well-vegetated, active and ecologically diverse succession of pedestrian-centric space between Development Parcels.

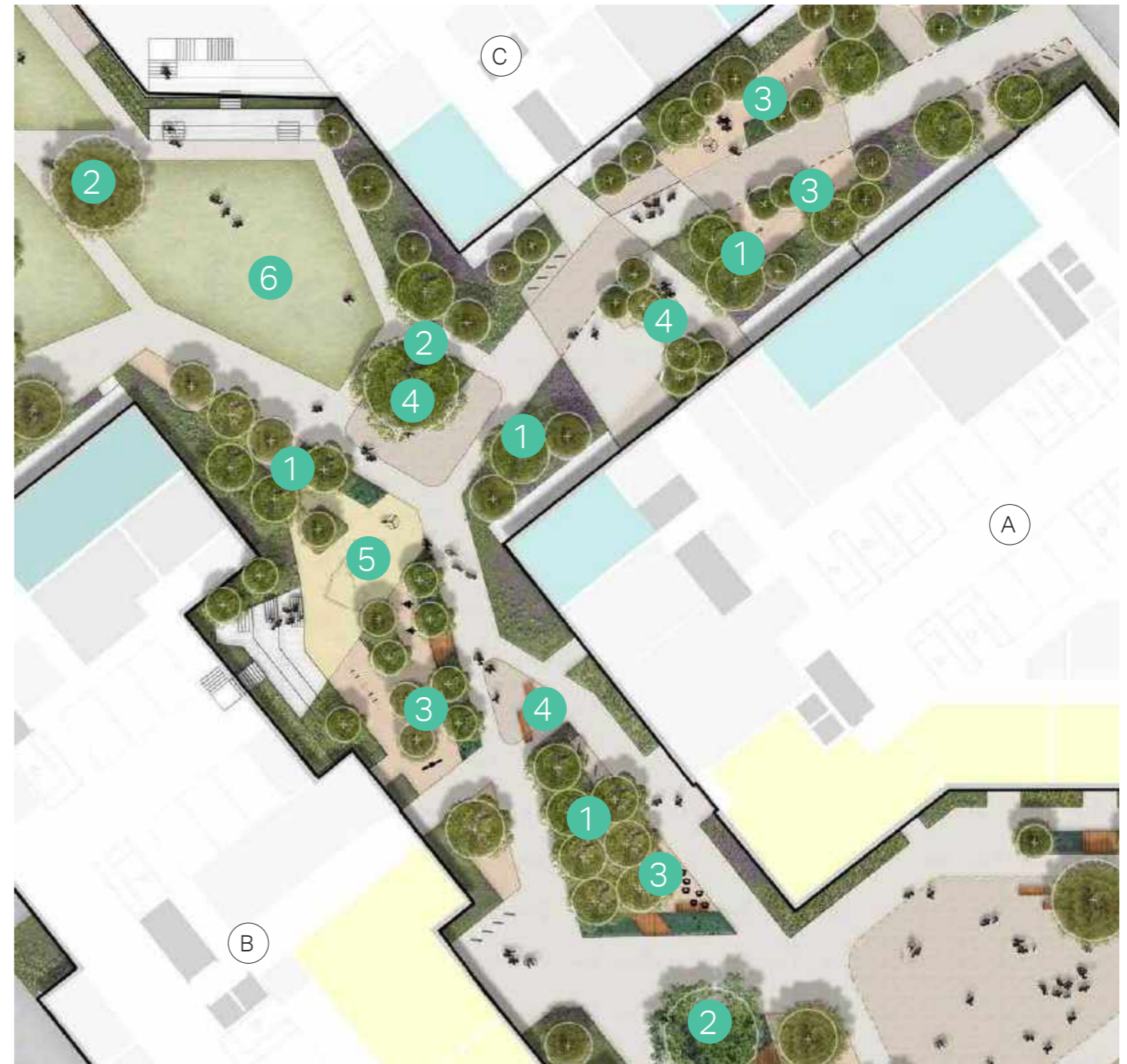
The Wood Way is envisaged as the principal ecological heart of the development. Using an informal “crinkled” design language, the Proposed Development seeks to create a sequence of spaces with a variety of atmospheres: hard and soft, sunny and covered, open and intimate. Conceived as a retreat from the surrounding urban context, the environment created is a relaxing, enjoyable and playable series of spaces that offer a variety of seating and play opportunities and are ecologically and environmentally sustainable. Resident balconies benefit from views over the woodland gardens, not only providing a green outlook for residents but playing an important role in passive surveillance of the public realm.

Visitors and residents can sit on the benches within the open spaces or individual seats within pocket spaces to observe users of the adjacent open spaces. Children and adults alike can explore the tales of the “woodland” by following the playful trails across the gardens. Materials are soft, with a traditional woodland feel – planting is dense and lush, multi-stem trees in clusters and groves create quiet, intimate social experiences, and naturally focused play items provide a range of physical challenges and play opportunities.

Key Principles

- Multi-species groves of deciduous and evergreen clear stem and multi-stem trees and species-rich shrub, grass and herbaceous planting;
- Raingardens to provide an on-site Sustainable Drainage System, where appropriate;
- Planting and tree groves to buffer the building interface where there are no access points. Planting and tree groves to co-ordinate with ground floor dwellings and provide a hedged buffer between the public realm and any residential interfaces;
- Large semi-mature clear stem deciduous and evergreen specimen trees to be used as way-finding mechanisms and to assist with semiotics;
- 5m wide non-linear pedestrian to double as fire egress path in the event of an emergency;
- Additional paths (min 1.6m wide) between tree and planting grove and dissecting the lawn area;
- Variety of surface materials and textures as appropriate to the defined space usage;
- Incidental timber play trails and natural play pockets located adjacent main path and within planting;
- Artistic and dedicated natural play areas adjacent to the main path with an appropriate area of safety surface.
- Nodes of seating and community tables catering for a variety of small group sizes, including those with children;
- Educational components regarding planting, ecology and wildlife within groves;
- Wildlife habitats within groves and within sight of pathways;
- Terraces landscaped to podium level to include planting and a variety of seating options to take advantage of their south-facing aspect;

- | | | | |
|---|---------------------------|---|---------------|
| 1 | Multi-species tree groves | 4 | Seating nodes |
| 2 | Marker trees | 5 | Play area |
| 3 | Play pockets and trail | 6 | Playable lawn |





DIVERSITY IN PLANTING AND TREES



RESPITE SPACES



NATURAL PLAY



Concept sketch of Wood Way

Cricklewood Lawn

Cricklewood Lawn is a high quality green open space for recreation and gathering.

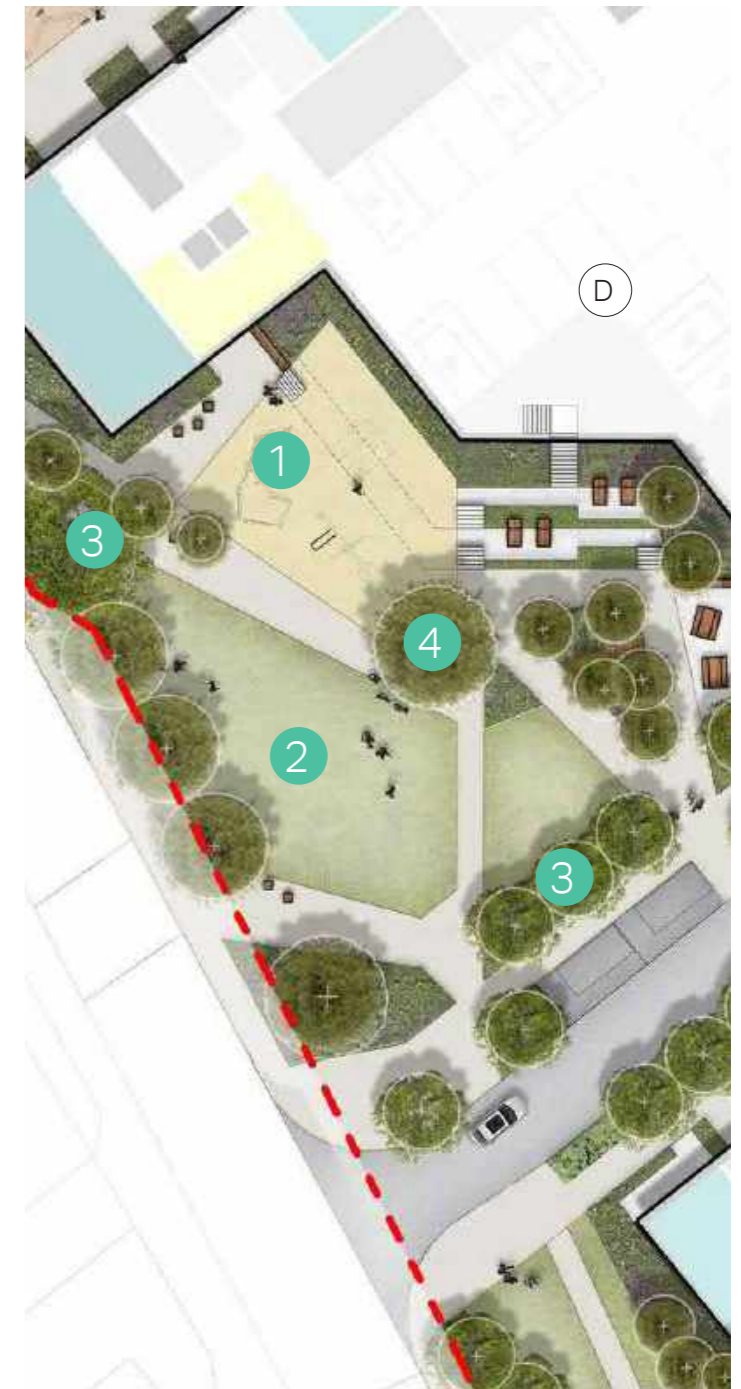
Cricklewood Lawn is composed of two levelled open lawn spaces flanked by the podium residents gardens. Architecture and landscape work together to provide an integrated transition between the podiums and public realm, guiding the eye to the planted podium gardens and framing the public space with a green and soft elevation. Planted terraces with seating platforms are proposed, allowing for the public to sit, relax in the sun and socialise in small groups while overlooking the playable lawn.

These lawn areas are dissected by footpaths and provide spaces for active recreation and lawn games, with play-on-the-way elements scattered along the paths and edges of the open space. Together they seek to provide a different typology of play complementing the wilder planted landscape of the Wood Way and sloped landscape of Cricklewood Green.

The northern-most lawn area is envisaged as the Community Heart of the Proposed Development. A destination play area is proposed near the Centre and away from the vehicular access. This artistic and dedicated prescriptive play area creates a node of activity against the planted podium terraces. Large semi-mature clear stem trees are used as markers in the landscape, while smaller specimens border these green spaces to create shelters nodes of seating and community tables.

Any future RMA should demonstrate that the landscape proposals for this character area are designed with enough flexibility to accommodate a raised table pedestrian connection/crossing over the Depot Approach to Kara Way Playground.

- 1 Play area adjacent to internal community space
- 2 Large playable lawn
- 3 Planted edges with play and seating
- 4 Marker trees





MULTI-DIRECTIONAL PATHS



MULTI-PURPOSE LAWN



DESTINATION PLAY AREA



LARGE NATURAL PLAY ELEMENTS



Concept sketch of Cricklewood Lawn

Key Principles

- An area of species-rich amenity lawn adjacent internal community space, the Depot Approach and Kara Way Playground;
- Paths (min 1.6m wide) dissecting the lawn area;
- Nodes of seating and community tables catering for a variety of small group sizes, especially those with children;
- Dedicated sheltered picnic area incorporating community tables adjacent lawn area;
- Large semi-mature clear stem deciduous and evergreen specimen trees to be used as way-finding mechanisms and to assist with semiotics;
- Multi-species deciduous and evergreen groves of trees and species-rich shrub, grass and herbaceous planting to frame green space and roadways;
- Raingardens to provide an on-site Sustainable Drainage System, where appropriate;
- Planting and tree groves to buffer the building interface where there are no access points. Planting and tree groves to co-ordinate with ground floor dwellings and provide a hedged buffer between the public realm and any residential interfaces.

The Rail Side

The Rail Side provides service and vehicular access to buildings adjacent to the rail corridor.

The Rail Side provides the primary service and vehicular entrances into the Proposed Development. The design approach of the Rail Side is key to ensure it does not develop a traditional 'back of house' feel and aesthetic. The main entrance point gives access to Blocks A,C, and D, through a delineated carriageway flanked by raingardens and clear-stem street trees.

Along the rail corridor, the dense collection of large existing trees are retained to maximise the green aesthetic of the space and create an ecological corridor connection to Network Rail land. A number of bug hotels and loggeries make this green corner an ecological asset for the whole site. The design of these spaces should ensure a level of activation through vehicular movement, whilst proposed lighting should help make the space feel safe and secure, especially at night.



Key Principles

- 6m wide hardstand surface carriageway with flush kerb delineation of parking bays and pedestrian areas.
- Carriageway should accommodate two-way traffic, cyclists and servicing vehicles (including taxis). Carriageway should avoid a traditional upstand kerb and macadam road typology.

- Selection of semi-mature 3m clear stem deciduous and evergreen specimen trees to flank carriageway in traditional street tree typology.
- Planting and tree groves to buffer the building interface where there are no access points. Planting and tree groves to co-ordinate with ground floor dwellings and provide a hedged buffer between the public realm and any residential interfaces.



- Semi-mature clear stem deciduous and evergreen specimen trees and multi-stem tree groves with species-rich shrub, grass and herbaceous planting to screen adjacent rail corridor.
- Existing trees retained, where appropriate.
- A turning head at each termination of the roadway/hardstand to accommodate movement of a service vehicles.

Podiums and roof terraces

The private residents' shared amenity space is divided into two Landscape Character Areas. These present an opportunity to create spatial variety within the private platform.

Podium Glades

The Podium Glades are high quality, peaceful and communal areas for residents and guests overlooking the Wood Way and landscaped public realm at ground floor. The blocks have been orientated to ensure that the-resident's amenity spaces at podium and roof level receive good amount of daylight/sunlight throughout the year at various times of the day.

Communal Roof Terraces

The Communal Roof Terraces are high quality, green, shared amenity spaces for the residents of each block at roof-top level.



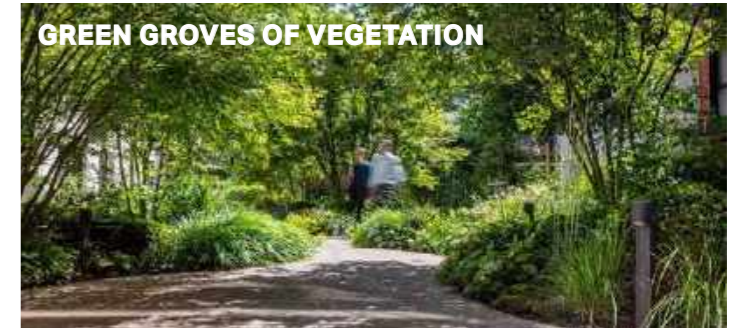
Podium Glades

The Podium Glades are high quality, peaceful and communal areas for residents and guests overlooking the Wood Way and landscaped public realm at ground floor.

The Podium Glades courtyards have been designed to be a green residential oasis with an emphasis on providing visual amenity and semi-private space for residents and guests. The design of these glades extends the Wood Way ambience and design typology to an elevated level. This creates a positive connection between people and ecology, establishing verdant spaces to be enjoyed, where both people and ecological habitat can thrive together.

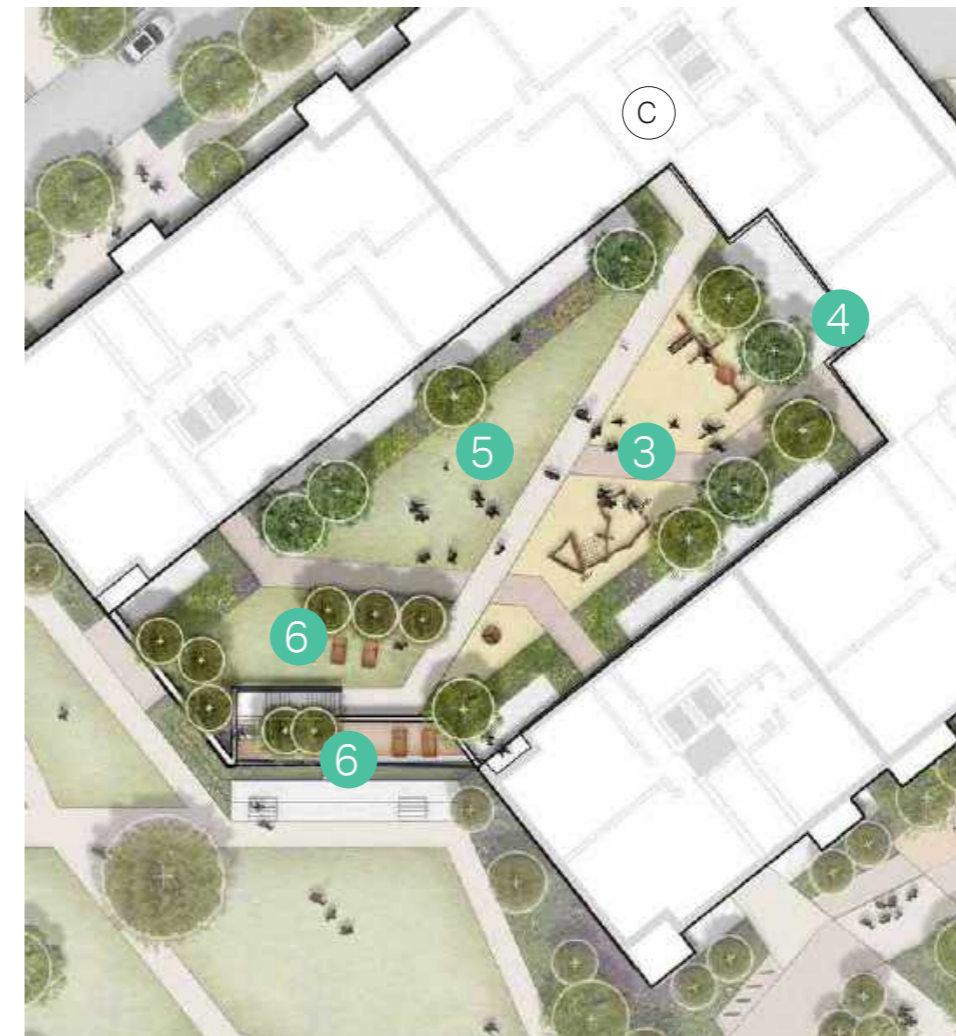


- 1 Terrace leading up to residents' podium gardens
- 2 Gathering space with shared table and seating
- 3 Timber-centric residential play area
- 4 Residential terraces
- 5 Amenity lawn
- 6 Respite and alcove spaces



The terraces which connect the ground floor to these podium landscapes create distinct and unique transition spaces and provide opportunities for observation and play. As a distinct threshold in the landscape, they seek to distinguish the active and vibrant community spaces from the more residential focused Podium Glades.

Central to the design of these glades is the provision for playful, social, recreational spaces embedded in lush green planting. Planters containing wildlife habitats and playful trails edge the Podium Glades to promote a whimsical feel, whilst clusters of trees provide privacy from onlookers and create a visual connection to the ground floor landscape.



Key Principles

- Terraces landscaped to ground level to include planting, play components and a variety of seating options to take advantage of their south-facing aspect.
- Step access provided to ground level, where appropriate.
- Multi-species deciduous and evergreen multi-stem groves of trees and species-rich shrub, grass and herbaceous planting.
- Dedicated seating areas with community tables sheltered within vegetation to cater for a variety of users and activities.
- Prescriptive play areas near the centre of podiums with an appropriate area of safety surface.
- Paths (min 1.5m wide) connecting shared residential entrances to podium steps and terraces
- Incidental timber play trails located adjacent paths and within planting.
- Variety of surface materials and textures as appropriate to the defined space usage.
- Materials to be permeable and coordinated with SuDs and drainage strategies.
- Wildlife habitats within planting.
- Planting to co-ordinate with podium-level units and provide a hedged buffer between the communal area and any residential interfaces.
- Residential interfaces to be walled/fenced from communal area to provide privacy.

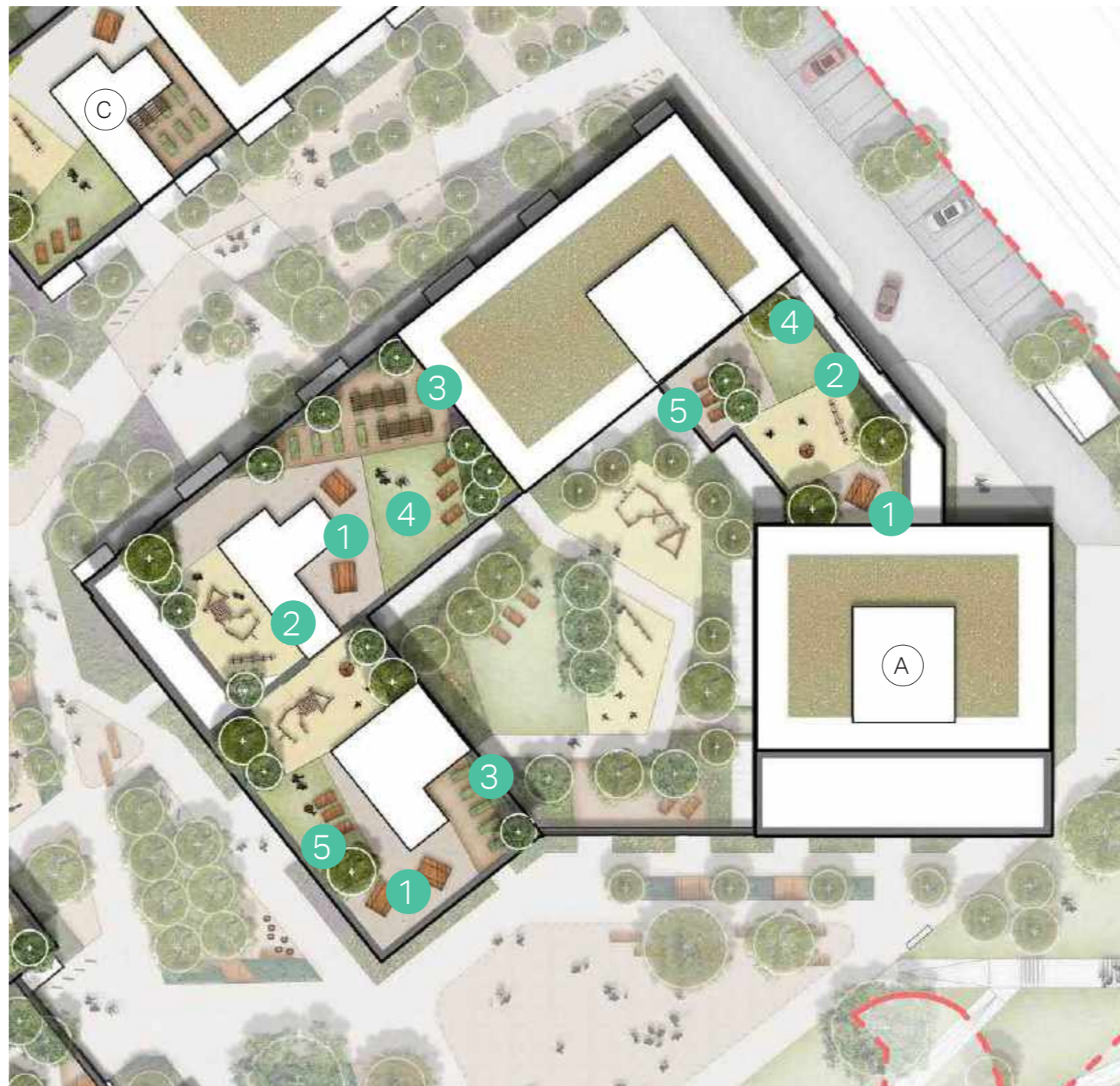
Communal roof terraces

The Communal Roof Terraces are high quality, green, shared amenity space for the residents of each block at roof-top level.

The communal roof terraces provide an opportunity for additional recreation and relaxation for residents through the provision of a number of elevated gathering and play areas. With views overlooking North London and beyond, these terraces provide a desirable alternative amenity space for residents to enjoy. They have places for small groups to gather, residential organisations to assemble, children to play, or individuals to relax.

As platforms to nurture community through a diversity of resident focused spaces, the roof terraces should also house a number of self grow beds for small scale horticultural production and to help foster a community spirit. Raised planters throughout the spaces should help to promote a garden feel, whilst trees provide privacy from onlookers and create a visual connection to the podium and ground floor landscapes.

Appropriate mitigation measures should be incorporated to control wind micro-climate to ensure that these areas are comfortable for use.



Key Principles

- Raised planters filled with species-rich shrub, grass and herbaceous planting and to provide sufficient soil volume to support healthy plant growth.
- A selection of multi-species deciduous and evergreen multi-stem trees.
- Large areas of species-rich lawn with reclining seating at edges located to maximise aspect and views.
- Variety of hardscape surface materials as appropriate to the defined space usage.
- Prescriptive play areas with an appropriate area of safety surface.
- Raised 'self-grow' beds in lineal arrangements with timber arbours for climbing species.



COSY CONTEMPLATION SPACES



SELF-GROW AND RESIDENT HORTICULTURE



RESIDENTIAL PLAY SPACE



COMMUNITY SOCIAL SPACES

- 1 Community tables and gathering space
- 2 Timber-centric play area
- 3 Self-grow beds and pergolas
- 4 Amenity lawn
- 5 Respite and alcove spaces

- Dedicated seating areas with community tables and barbecues sheltered within vegetation to cater for a variety of users and activities.
- Rooftops to be privately managed and restricted access to residents only.

6.9 Biodiverse roofs

Biodiverse roofs provide additional ecological green spaces which are non-accessible to residents but thriving in biodiversity.

Initiatives such as the All London Green Grid, the GLA's policies on Biodiversity and Access to Nature, coupled with identification of priority habitats in local authorities Biodiversity Actions Plans (BAP) are driving an important and significant agenda of ecological focused spaces to support delicate and threatened urban ecologies.

Roofs are an excellent opportunity to provide a variety of habitats and support a range of local birds and insect species.

The proposed roofs are a mosaic of nectar-rich low maintenance planting and self seeded vegetation interspersed with bare ground, stones, sand, rubble, and logs.



6.10 Play strategy

The Proposed Development has been designed to be inherently playful in nature, yet include traditional play areas and elements as mechanisms for activation. A series of integrated playable routes and elements cement play into the development, providing opportunities to jump, run and scramble.

A large portion of play quantum is provided in the ground floor public realm with additional play areas being included in the podium and roof terrace landscapes exclusively for residents. This allows for play provision to not be entirely restricted to residents of the Proposed Development but rather, provide a certain quantum of playspace that can be used and enjoyed by the wider neighbourhood.

Play should be naturalistic and opportunistic in appearance. Timber is used as a central play element material with non-timber elements being of subtle and muted colour, form, and texture, unless it is an element of destination play..

Health and well-being for all ages is promoted through the use of play-along-the-way mechanisms and equipment that provide a range of physical challenges that can be used by a variety of age groups, including adults. These integrated play opportunities activate the Site in nodes embedded within a green infrastructure network. Passive surveillance is maintained and play areas seeks to complement any flexible commercial uses by providing a range of social and educational opportunities.

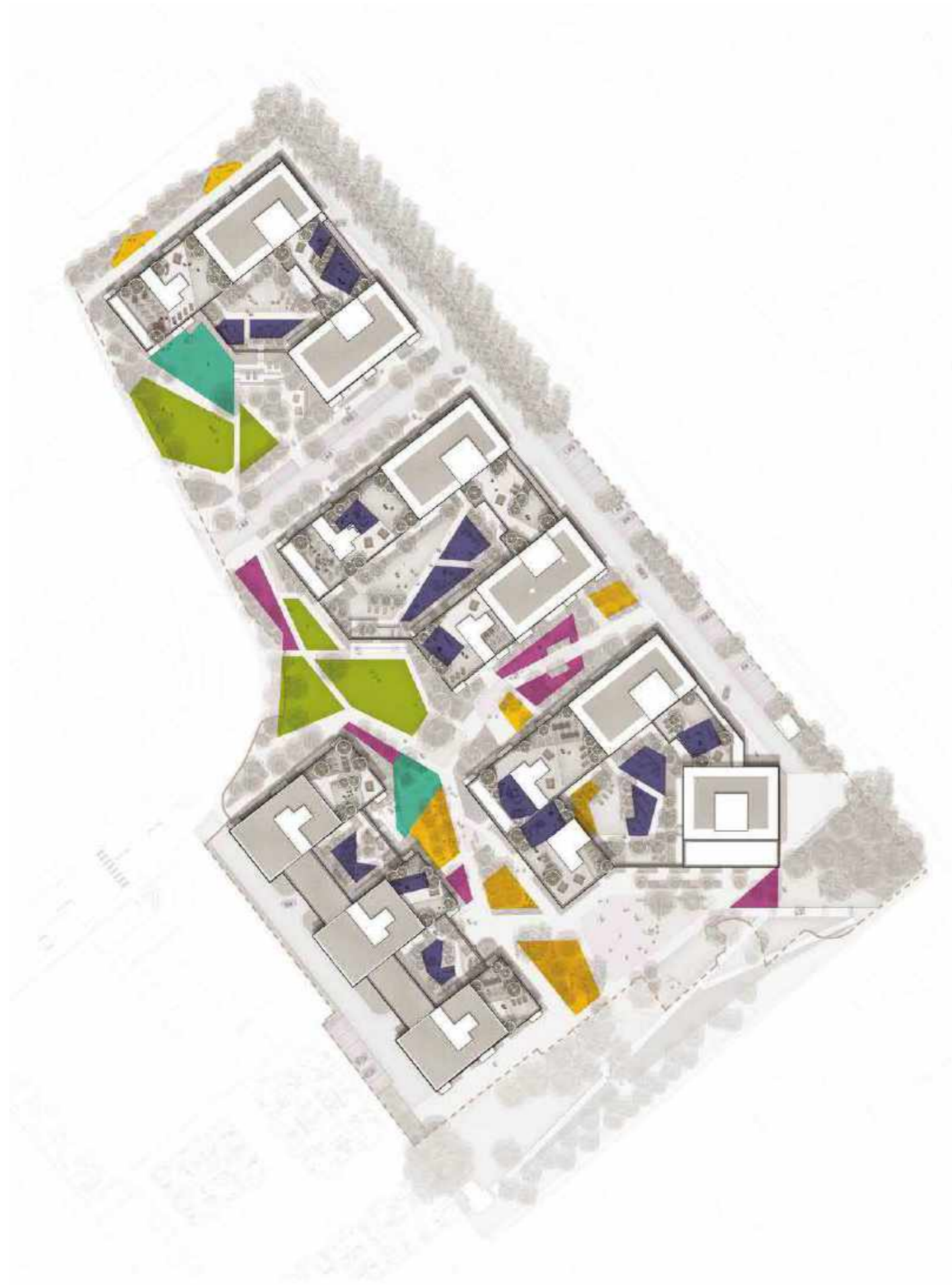


Requirement

0 to 4 YRS	1727.0 m ²	50.2 %
5 to 11 YRS	1182.0 m ²	34.4 %
12 to 15 YRS	346.0 m ²	10.1 %
16 + YRS	183.0 m ²	5.3 %
TOTAL	3437.8 m²	100 %

Provided

0 to 4 YRS	1742.79 m ²	+ 0.97 %
5 to 11 YRS	1298.94 m ²	+ 9.97 %
12 to 15 YRS	381.02 m ²	+ 10.39 %
16 + YRS	190.99 m ²	+ 4.88 %
TOTAL	3613.74 m²	+ 5.12 %



DESTINATION PLAY



ALONG THE WAY



LAWN PLAY



NATURAL PLAY



PODIUM / ROOFS



Illustrative sketch view of the playspace and communities facilities towards the north of the Site, linking to the existing Kara Way playground





6.11 Hard landscape strategy

The material palette of hard surfaces takes its cue from streets and materials used locally around the Site. This is a considered effort to embed the aesthetic of the Proposed Development within the context of the wider Cricklewood area and form a coherent public realm which links to the architectural materiality of the Proposed Development and wider context presenting a unified aesthetic.

Creating a unified visual aesthetic is integral to the selection of hard materials (and furniture). Materials should be high-quality, durable and seek to create a cohesive site wide design language to improve legibility and way-finding. Materials are to be robust in nature; consideration of their long-term performance and appearance over time is important.

The inclusion of textural and colour variations defines different programmes and functions within the landscape. Utilising porous and permeable materials where most appropriate promotes sustainability.





6.12 Soft landscape strategy

The Proposed Development enhances the overall green infrastructure network by creating a series of interconnected green spaces, including rain gardens, biodiverse amenity beds, green open space and both linear and clustered tree arrangements.

The soft landscape strategy seeks primarily to create a strong, well-vegetated site character that forms a rich and immersive environment. Species have been chosen from a diverse palette to match the anticipated micro climate and drainage strategy of the proposed planting area. An integrated and considered soft landscape palette seeks to clearly define spaces, soften the appearance of the Proposed Development, promote sustainable drainage, and enhance ecological diversity, as well as provide visual interest and colour throughout the seasons.

Specific consideration has been given to species which are nectar rich, native and provide fruit and seeds for wildlife and foraging humans.

The planting palette is non exhaustive and should be developed and agreed via conditions pursuant to a planning consent.

GENERAL PLANTING

A dynamic palette with variation in textures and heights. The species range from 300mm to 1m and the colours complement the distinctive leaves of the marker trees.



- Hebe salicifolia*
- Miscanthus sinensis*
- Deschampsia cespitosa*
- Carex morrowii* 'Irish Green'
- Luzula nivea*
- Gaura lindheimeri*

RAINGARDENS

A palette consisting of plants that tolerate inundation and moist environments and provide seasonal colour and a variety of textures.



- Iris sibirica*
- Carex pendula*
- Iris pseudacorus*
- Campanula glomerata*
- Miscanthus sinensis*
- Juncus effusus*

WOOD WAY

A lush planting palette to create an immersive environment with soothing colours and textures. The species are shade tolerant and evoke woodland flora.



- Dryopteris filix-mas*
- Luzula nivea*
- Alchemilla mollis*
- Ajuga reptans* 'Alba'
- Osmunda regalis*
- Silene dioica*

PODIUM GLADES

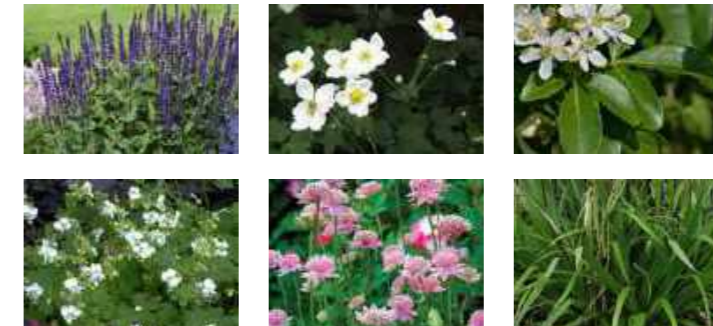
A palette of glossy, light reflecting plants that tolerate shade and dappled light while providing a variety of colours.



- Polypodium vulgare*
- Galium odoratum*
- Anemone nemorosa*
- Deschampsia cespitosa*
- Luzula sylvatica*
- Geranium macrorrhizum*

ROOF TERRACES

A durable and colourful palette of soft dense vegetation to provide a strong and robust planted edge to the communal roof terraces.



- Salvia nemorosa* 'Caradonna'
- Geranium macrorrhizum*
- Anemone x hybrida* 'Honorine Jobert'
- Astrantia major rosea*
- Choisya ternata*
- Carex pendula*



6.13 Tree strategy

Retained trees, along with the addition of several new trees is an important mechanism utilised in the landscape to enhance biodiversity, assist in way-finding, contribute to the wider green infrastructure network and create a high quality, connected and distinctive public realm.

Tree species have been selected on the basis of:

- Suitability for the scale of the space and its location;
- Way-finding strategy with marker trees;
- Suitability to co-ordinate with character area planting typologies e.g.: evoke woodland-esqe character
- Provision of food sources for local fauna; and
- Seasonal interest and autumn/spring colour and to diversify the age range of species for enjoyment for this generation and the next.

The tree palette is non exhaustive and should be developed and agreed via conditions pursuant to a planning consent.

WAY-FINDING TREES						<p><i>Liquidambar styraciflua</i> <i>Gleditsia triacanthos</i> <i>Acer platanoides</i> <i>Alnus cordata</i> <i>Quercus robur</i></p>			
	GROVES / CLUSTERS						<p><i>Betula pendula</i> <i>Betula pubescens</i> <i>Acer campestre</i> <i>Sorbus aucuparia</i> <i>Corylus colurna</i></p>		
		SPECIMEN TREES RAINGARDENS						<p><i>Alnus glutinosa</i> <i>Betula nigra</i> <i>Betula utilis var. jacquemontii</i> <i>Amelanchier x grandiflora</i> <i>Alnus incana</i></p>	
			PODIUM GLADES ROOF TERRACES						<p><i>Amelanchier lamarckii</i> <i>Carpinus betulus multi stem</i> <i>Cornus kousa</i> <i>Cornus mas</i> <i>Acer palmatum</i></p>



6.14 Street furniture

Street furniture, like proposed hard materials, is envisioned as having a unified visual aesthetic that utilises a cohesive palette of materials, fixings and equipment elements across all character areas. The furniture selection seeks to co-ordinate with the existing palette of furniture in the surrounding context and align with the appearance of the proposed built form.

Utilising a timber-centric palette of furniture, the products will be robust, with the longevity of material and finish being considered upon selection, along with the long term appearance and performance.

SEATING ELEMENTS



Urban type furniture in Arboretum Place, Cricklewood Green, Cricklewood Lawn and Communal Roof Terraces



Timber-centric type furniture in Wood Way and Podium Glades



Community tables, recliners and other ornamented seating of similar palette

BINS / BOLLARDS/ETC



Timber-centric (or ornamented) street furniture

ECOLOGICAL



Creative and colourful bird boxes, bug hotels, ecological homes and wildlife learning interventions

6.15 Lighting

The lighting strategy intends to highlight key elements and features throughout the Proposed Development, illuminate pedestrianised areas during dark hours for safety, and be sensitive to the existing environment and local context. Safe routes are provided along key pathways across the Site to create a greater sense of security and personal safety.

Lighting is integrated within street furniture (e.g. way-finding lighting within paving and lighting of feature elements such as uplighting to trees etc.) to provide a high-quality and elegant aesthetic to enhance visual value.

Proposed lighting should mitigate any unnecessary stray light or sky spill, with the integration of all lighting being sympathetic to the local context and the needs of surrounding residential properties.

Consideration has been given to the needs of wildlife; some areas of planting are to be intentionally left un-illuminated. Less illuminated areas of vegetation and tree groves may provide flight paths for bats and birds and nest areas for smaller invertebrates.



Lighting highlights key landscape elements and provides illumination on the paved ground



Tree and seat uplighting provides a high quality and elegant aesthetic

6.16 Signage and way-finding

The way-finding strategy utilises a consistent palette of materials and signage forms to define spaces, orientate users and contextualise programme. Thresholds and a change in character or usage is denoted within the Site through the use of paving variation and a change in planting typologies. These seek to distinguish areas of relaxation from areas of activation, and differentiate areas of gathering from areas of solitude.

A clutter-free, clear, pedestrian environment is created through the use of consistent forms and materials. All signage within the Site, unless statutory, should be aligned visually and materially with the other elements of proposed furniture, including play equipment.

Trees are used as a primary mechanism of way-finding, along with orientation and interpretation signage. The surrounding streets are punctuated by mature trees that act as way-finding markers. Utilising marker trees with 'way-finding characteristics', - colour, height and size - seeks to link with and enhance the existing network of trees beyond the Site and increase pedestrian legibility throughout the Proposed Development.



Orientation and interpretation signage co-ordinates with other on-site elements/furniture/lighting



Ecological learning mechanisms are used in the Wood Way and Podium Glade landscapes





Adjacent Sketch view from a roof terrace of Building B looking onto the town square / civic space and Wood Way



7.1 Access principles

This chapter of the Masterplan Design and Access Statement aims to provide a clear description of how access principles have been incorporated into the masterplan, informing future RMAs.

The principles consider how the users of the Proposed Development will access, and be guided through the individual buildings and wider development, without discrimination or limitation. It considers, but is not limited to, the access and circulation needs of a wide range of people including the elderly, the disabled and parents with young children.

An inclusive environment does not attempt to meet every single need of every single user, but by considering people's diversity, inclusive environments can break down barriers and exclusion and will often achieve superior solutions that benefit the wider community.

Legislation and guidance can often result in 'special' provision being made for disabled people rather than their needs being integrated with all other users.

Inclusive design should extend beyond the final built product and should be considered at every level thereafter, including within the context of the management and operation of the buildings, throughout the duration of their life-cycle.

The masterplan has been designed to be as inclusive as possible so that it can be comfortably and independently used by residents, visitors, people working in and visiting the development and the wider community.

The masterplan and Illustrative Masterplan have considered principles of inclusive design and the requirements of all users, (including those with mobility impairments, visual impairments, deaf, elderly, and children); using a journey around the building as follows:

- Arrival at the Site;
- Approaches to the buildings;
- Entrance ways;
- Step-free access;
- Horizontal and vertical circulation;
- Access to facilities;
- Typical residential layouts; and
- The emergency evacuation strategy.





7.2 Legislation, standards and guidance

The masterplan and Illustrative Masterplan have been designed to ensure compliance with the relevant legislation, standards and guidance, including:

- The Equality Act 2010;
- National Regulations: The Building Regulations 2010, Approved Document M (Access to and use of buildings) Volume 1: Dwellings, HM Government, 2015 edition;
- The Building Regulations 2010, Approved Document M (Access to and use of buildings) Volume 2: Building other than dwellings, HM Government, 2015 edition;
- The Building Regulations 2010, Approved Document K (Protection from falling, collision and impact), HM Government, 2013 edition;
- The Building Regulations 2010, Approved Document B (Fire safety) Volume 1: Dwellinghouses, HM Government, 2006 edition incorporating 2010 and 2013 amendments;
- Approved Document B (Fire safety) Volume 2: Buildings other than dwellinghouses, HM Government, 2006 edition incorporating 2010 and 2013 amendments.

Best Practice

- British Standard 8300:2009 (Amended 2010) Design of Buildings and their Approaches to Meet the Needs of Disabled People - Code of Practice, British Standards Institution, 2010;
- British Standard 9999:2008 Code of Practice for Fire Safety in the Design, Management and use of Buildings, British Standards Institution, 2008.

National Planning Policy

- National Planning Policy Framework (2019);
- National Planning Practice Guidance (NPPG) (2019);

London Planning Policy

- Adopted London Plan (as amended), 2016; The London Plan: The Spatial Development Strategy for London (2016); and
- Housing Supplementary Planning Guidance, London Plan 2016 Implementation Framework, March 2016.

Local Planning Policy

The adopted Development Plan for the London Borough of Barnet sets out the planning policies for making planning decisions. The Development Plan consists of the following documents:

- LBB Core Strategy Development Plan Document (September 2012);
- LBB Development Management Policies (DMDP) (September, 2012);
- LBB Unitary Development Plan "13 saved policies" for Brent Cross and Cricklewood' (UDP) (May, 2006); and
- Cricklewood, Brent Cross and West Hendon Regeneration Area Development Framework (December 2005).

Emerging Policy

- Draft London Plan (2019) Intend to Publish Version;
- Barnet Draft Local Plan (January 2020).



APARTMENTS



7.3 Residential dwellings

The proposed Illustrative Masterplan aligns with Technical Housing Standards March 2015 and Building Regulations Approved Document Part M 2015 in order to maximise access for users with mobility, hearing and visual impairments.

The Illustrative Masterplan makes provision for the design of wheelchair accessible and wheelchair adaptable accommodation in line with the requirements of the London Plan and Approved Document M.

The Illustrative Masterplan facilitates no less than 90% of residential dwellings designed to M4(2) standards and up to 10% accessible to be M4(3) wheelchair accessible/adaptable dwellings; to be agreed with LBB.

Specific design features should be provided to further benefit all, but in particular the elderly and the physically impaired. This ensures the future wheelchair adaptations can be made to a home without great expense.

Dwellings should be designed in a cost effective way to ensure the Proposed Development will be flexible and appropriate to changing needs and are therefore sustainable.

7.4 Site wide access philosophy

A well designed public realm can contribute significantly to the quality of the built environment and play a key role in the creation of sustainable, inclusive, mixed communities.

The Illustrative Masterplan's public realm proposal follows a number of integrated principles which are in accordance with the guidelines set out in The Principles of Inclusive Design (CABE 2006).

Inclusive design which:

- Places people at the heart of the design process;
- Acknowledges diversity and difference;
- Offers choice where a single solution cannot accommodate all users' individual needs;
- Provides for flexibility in use, and;
- Provides buildings and environments that are convenient and enjoyable for everyone.

At future RMA stage, the public realm should be designed to maximise accessibility for all, with paved level access or ramp access directly off the pedestrian streets. Working with the existing site topography, the public realm should encourage free movement for the young and old, the disabled, the visually impaired and those using wheelchairs, prams and bicycles.

Main pedestrian circulation routes should be generous and uncluttered. The use of a limited palette of surface treatments and street furniture will present a coherent and legible public realm design.

In all public areas of the Illustrative Masterplan, internal and external, public and private, door widths and thresholds should be designed to allow for wheelchair access. Control buttons and handles to be of a size and shape that ensure maximum usability. Signage, use of colour and lighting design should ensure ease of navigation for the visually impaired.

Entrance lobbies should be well lit, sheltered and will be accessible via flush thresholds for wheelchair users.

Accessible car parking should be allocated and clearly marked.



APARTMENTS



Adjacent Sketch view of the landscaped public realm between buildings C and D looking towards Building B



CRICKLEWOOD LANE

SASSEM MARKET
MARGOT
MEL BOY
SCHEFF
ESKATIL
ESKATIL is a
community
marketplace
run by
TAV GOSWAMI
Collection from the
UK and
around the world
www.eskatil.com

Aspirational CGI render of the Proposed Development and enhancements to Cricklewood Green

8.1 Summary

This Proposed Development aims to regenerate the Cricklewood B&Q site - offering an opportunity to increase affordable housing by creating up to 1,100 homes - tackling the increasing strain on housing in Cricklewood. This is in addition to providing high quality, public green space within Cricklewood's town centre.

This ambitious Proposed Development aspires to:

- Create a new civic heart for Cricklewood, a destination and gathering place for local festivals and events, ensuring long term enjoyment by the local community, reflecting and building on Cricklewood local residents' civic aspirations and pride;
- Enhance and connect to the much-loved existing Kara Way play space and Cricklewood Green, improving both quantity and quality of public open space in Cricklewood town centre;
- Improve safety and security in and around the Site by promoting positive activity and passive surveillance;
- Establish a permeable and well-connected development, giving priority to pedestrians and cyclists by providing access routes that link into the town centre, train station and surrounding amenity spaces;
- Act as a catalyst for further regeneration and unlock the potential of the surrounding area;

- Improve the frontage along Cricklewood Lane, enriching the pedestrian experience towards the station and new town square;
- Contribute positively to the local townscape, adding legibility to the district centre and Cricklewood station;
- Improve the setting of the Cricklewood Railway Terrace conservation area by significant upliftment to the surrounding public realm;
- To ease traffic and congestion by enabling locals to take advantage of the close public transport links and cycling and walking routes; and
- Guarantee the redevelopment of a pivotal brownfield site that lies within the Brent Cross and Cricklewood Opportunity Area by maximising higher-density development and relieving development pressure on green field sites.

Regeneration brought by this Proposed Development will create an aspirational place in Barnet for families and working professionals to live. New residents will drive demand for better shops and facilities, providing a boost to the local economy that will benefit everyone in the area.

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